

Wharfedale Greenway

Burley-in-Wharfedale, Menston, Otley, Pool-in-Wharfedale



City of Bradford MDC
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Menston



May 2014

Foreword from the Leader of Otley Town Council and Chair of the Wharfedale Greenway Project

It gives me great pleasure to write the foreword to the Wharfedale Greenway Feasibility Study.

Otley Town Council has had a long relationship with Sustrans, and the desire to utilise the former railway line for leisure pursuits – cycling and rambling – has been at the forefront of planning over a number of years.

In the past twelve months it has been good to meet with colleagues in neighbouring Parish Councils (Pool-in-Wharfedale, Menston and Burley-in-Wharfedale) and with Sustrans and to find that there is a shared vision to link all of our communities – by what has become known as the Wharfedale Greenway Project.

All of our communities share a desire to improve facilities for cyclists and walkers, and this project has really caught the imagination – what could be better, in this, the year that the Tour de France comes to the beautiful Wharfe Valley.

This study – which we are hoping will be enthusiastically welcomed and endorsed by local residents – is not without its challenges. Clearly the major challenge is that of funding, but we hope that the self-evident partnership working that has helped produce this report will demonstrate to funders that this is a scheme whose time has come.

The Wharfedale Greenway Project I believe meets a number of objectives;

- Addressing health and well-being
- A major ‘environmental ‘ project
- Linking our communities, both to each other and to the wider transport network – particularly to local railway stations
- Focussing on partnership working (we really are stronger than the sum of our parts)
- Addressing accessibility

In commending this report I would like to thank Sustrans for its production, and for all of the partner organisations for their part in its delivery.

Town Councillor John Eveleigh, Council Leader, Otley Town Council



Introduction to the Wharfedale Greenway

Traffic free greenways are unique spaces for commuting, leisure trips and for experiencing wildlife whilst travelling by foot, bike, or wheelchair. A well maintained surfaced pathway within a green corridor, typically a disused railway line, wide enough to allow users to pass and where gradients are kept to a minimum to suit all levels of fitness. They are best understood by experiencing them first hand with nearby examples including the Aireborough Greenway (Yeadon to Guiseley), the Nidderdale Greenway (Harrogate to Ripley) and The Harland Way (Spofforth to Wetherby).

The Poole-in-Wharfedale, Otley, Menston and Burley-in-Wharfedale cycleway feasibility study was prepared by Sustrans in 2010 for Otley Town

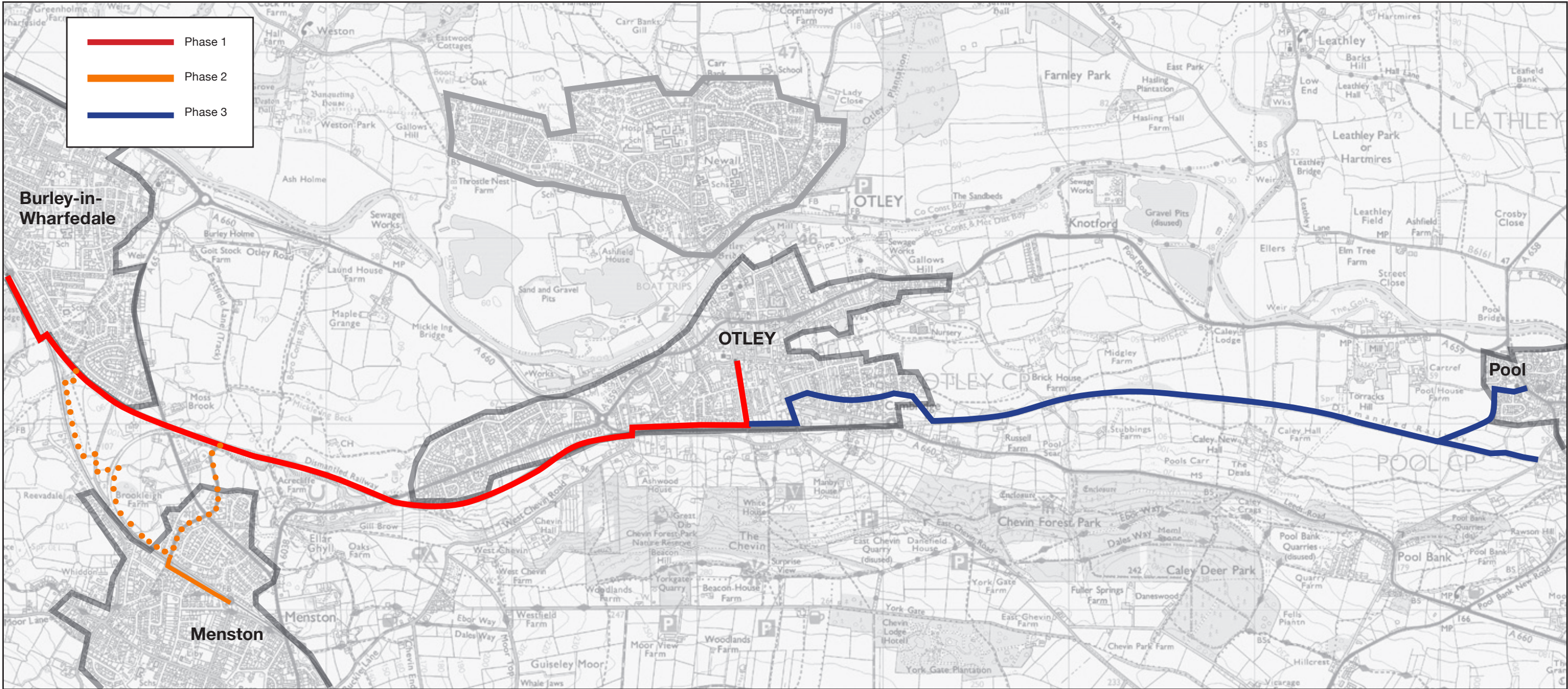
Council. The brief was to assess the scope for improving walking and cycling routes to, through and around the town. Unsurprisingly, the study highlighted the opportunity to create a series of greenways along extant sections of the former Arthington to Burley-in-Wharfedale railway (closed in 1965) to provide primary walk/cycle routes to Menston and Burley stations, as well as a planned new station at Arthington. The report was taken through a number of Town Council committee stages and Sustrans was asked to request comments from Leeds City Council cycle development officers, and notify private landowners affected by the proposals before undertaking any formal public consultation. The timing of the 2010 study was unfortunate in that it coincided with substantial cuts in public

funding and Leeds City Council requested time to complete work on urban-Leeds sections of the city's cycle network before reviewing further greenway proposals in the rural townships.

The announcement in early 2013 of the Tour de France Grande Depart from Leeds in July 2014, created a renewed acknowledgement of the interest in and importance of safe cycling across the whole City Region. In the autumn of 2013 a steering group was formed involving Otley Town Council, Burley-in-Wharfedale Parish Council, Menston Parish Council, Pool-in-Wharfedale Parish Council, Bradford City Council, Leeds City Council and Sustrans. There was a desire to see a safe route for walking and cycling linking the parishes together. It was decided

to follow the recommendation made in the 2010 report and put forward the old railway corridor, to be named the Wharfedale Greenway, as the proposed route.

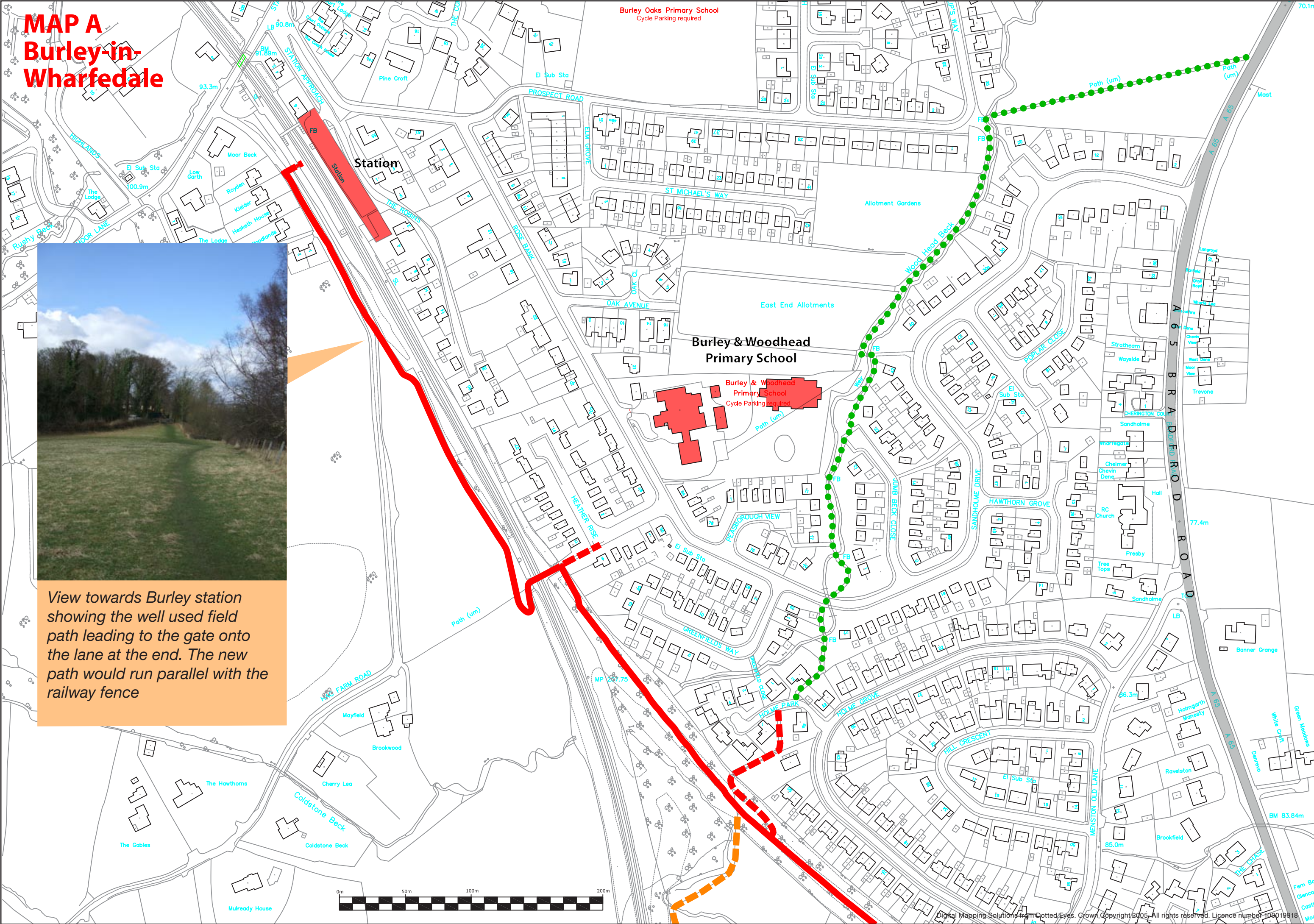
The steering group would now like to consult with the local community to determine the general level of support and to use the hopefully positive findings of this consultation to secure the funding required to deliver this amenity. There hasn't been any revisions to the original report but additional information is included beforehand which adds extra details and alternative options to the originally proposed route alignment and possible phases of construction.



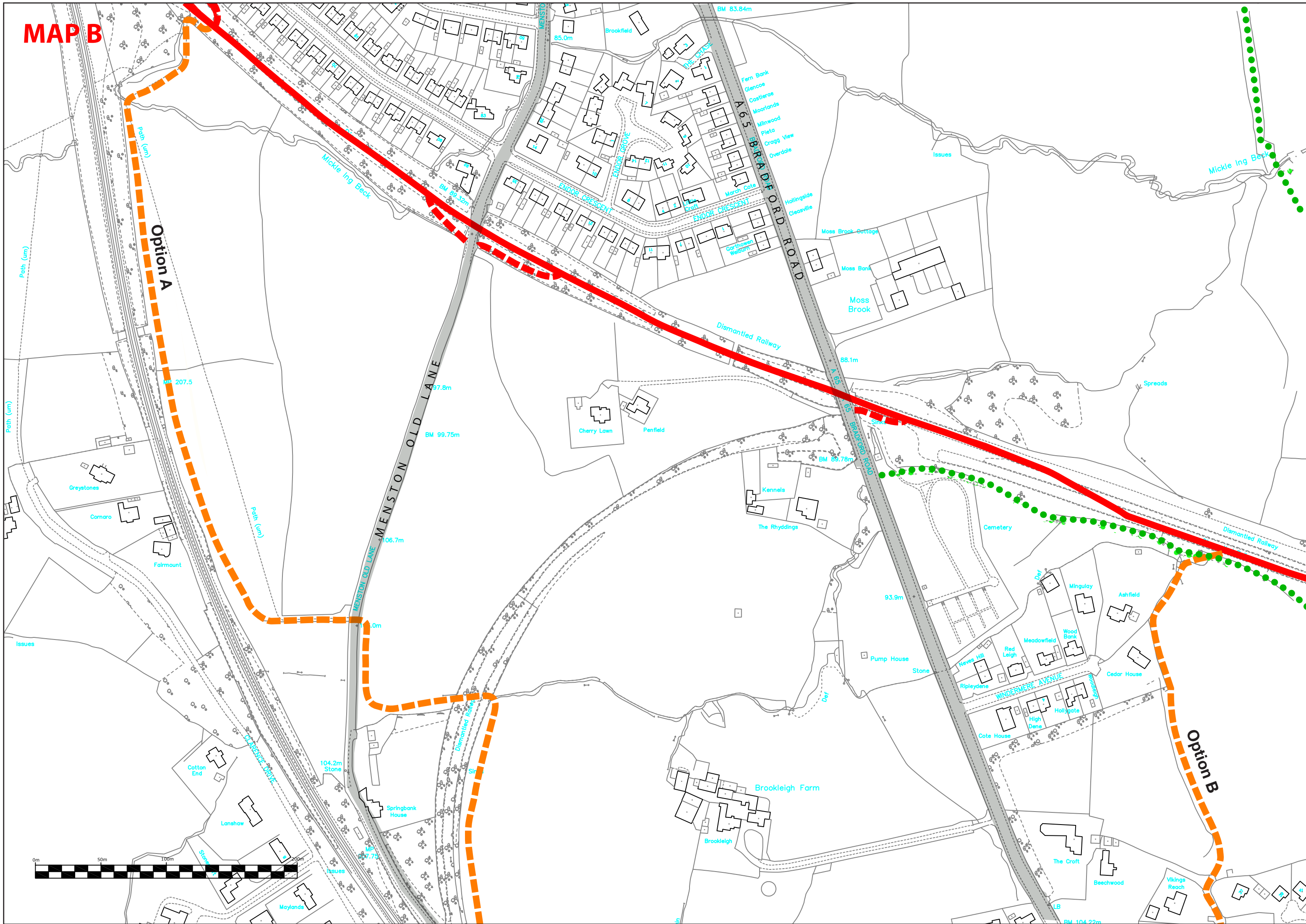
MAP A
Burley-in-Wharfedale



View towards Burley station showing the well used field path leading to the gate onto the lane at the end. The new path would run parallel with the railway fence



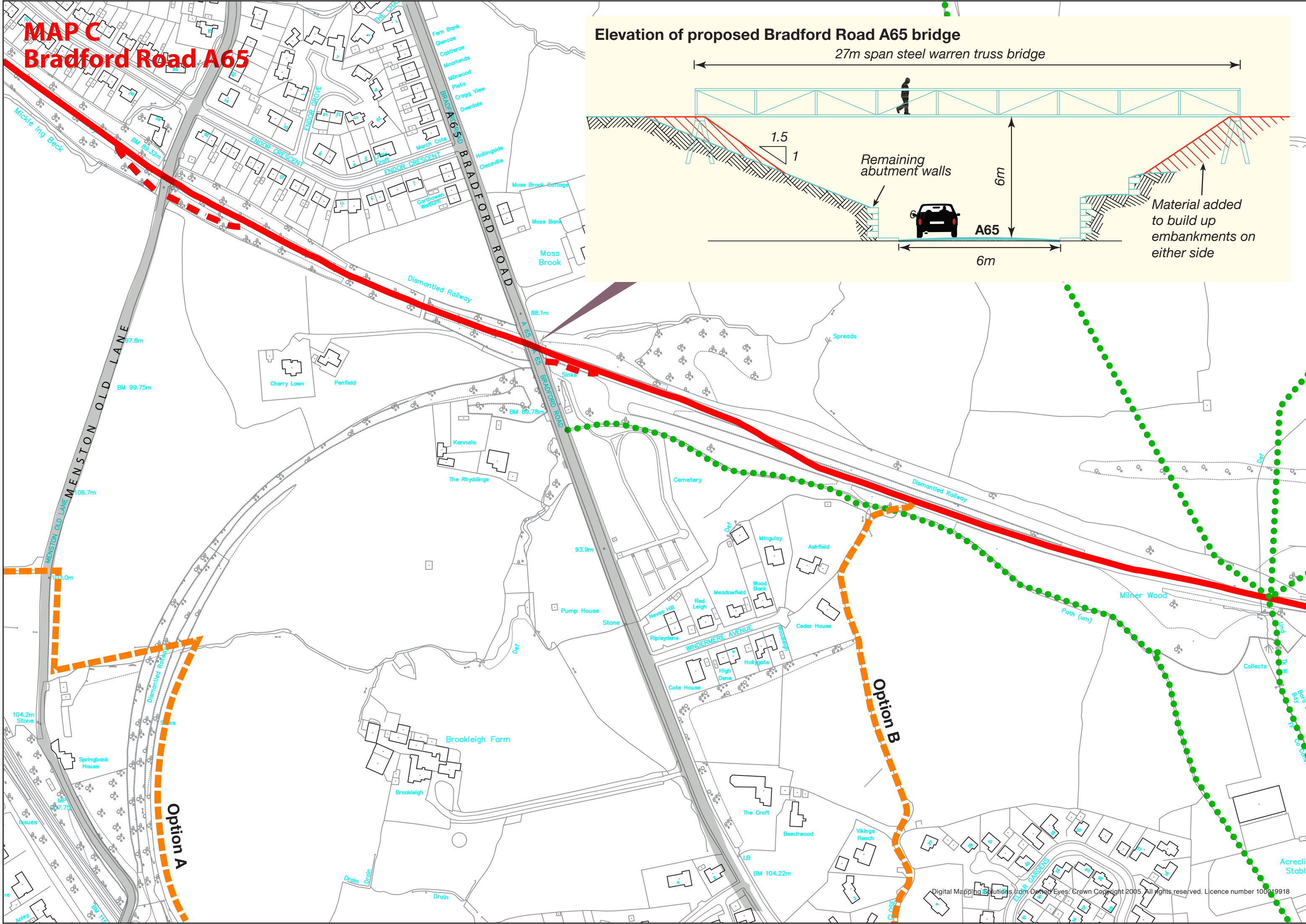
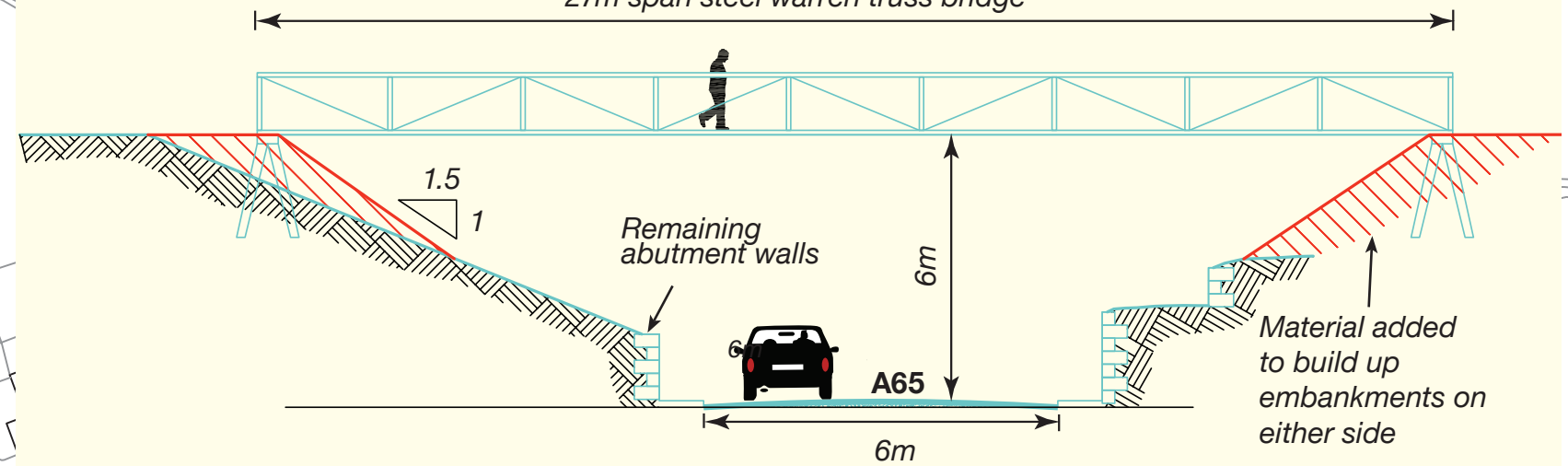
MAP B



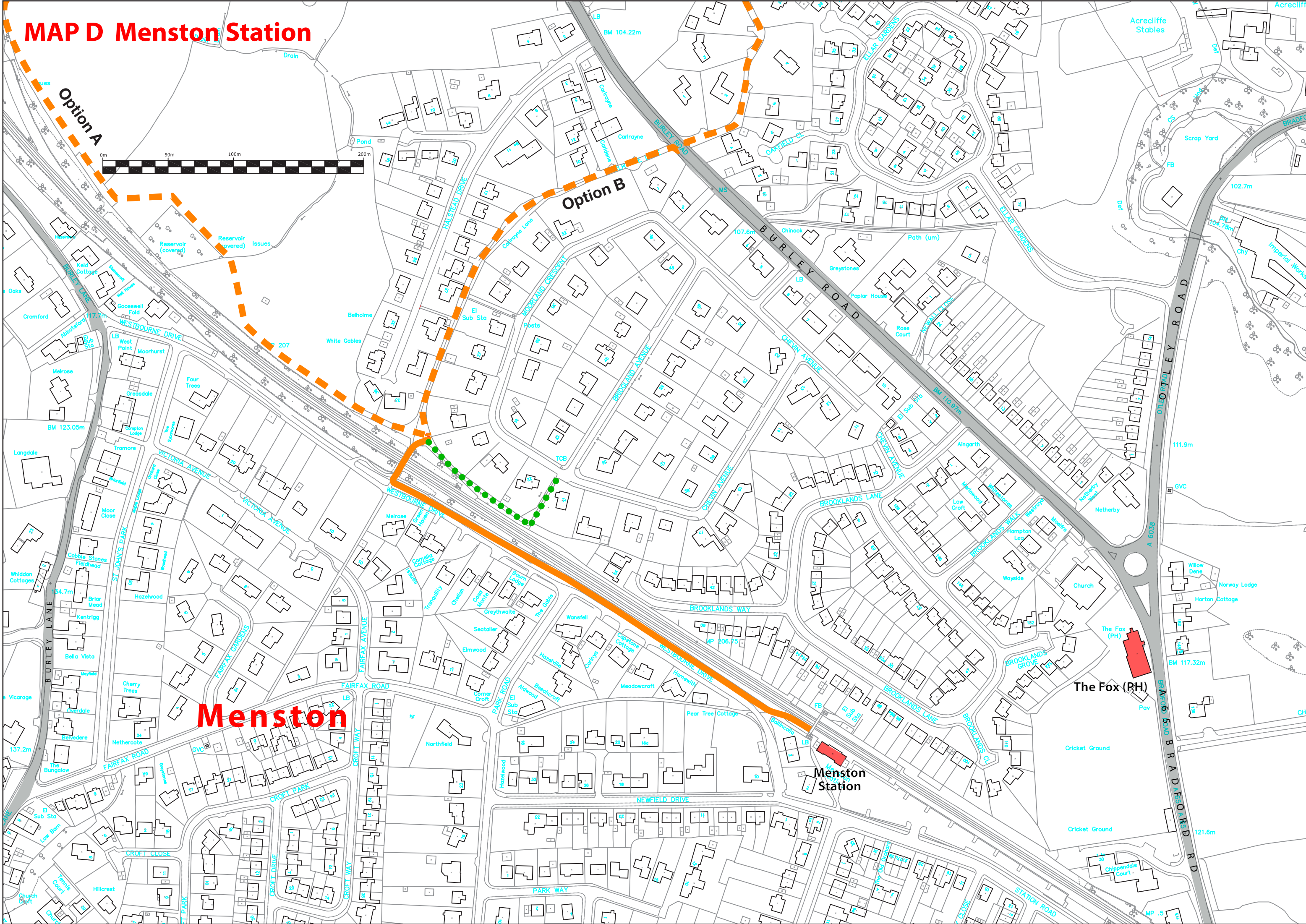
MAP C Bradford Road A65

Elevation of proposed Bradford Road A65 bridge

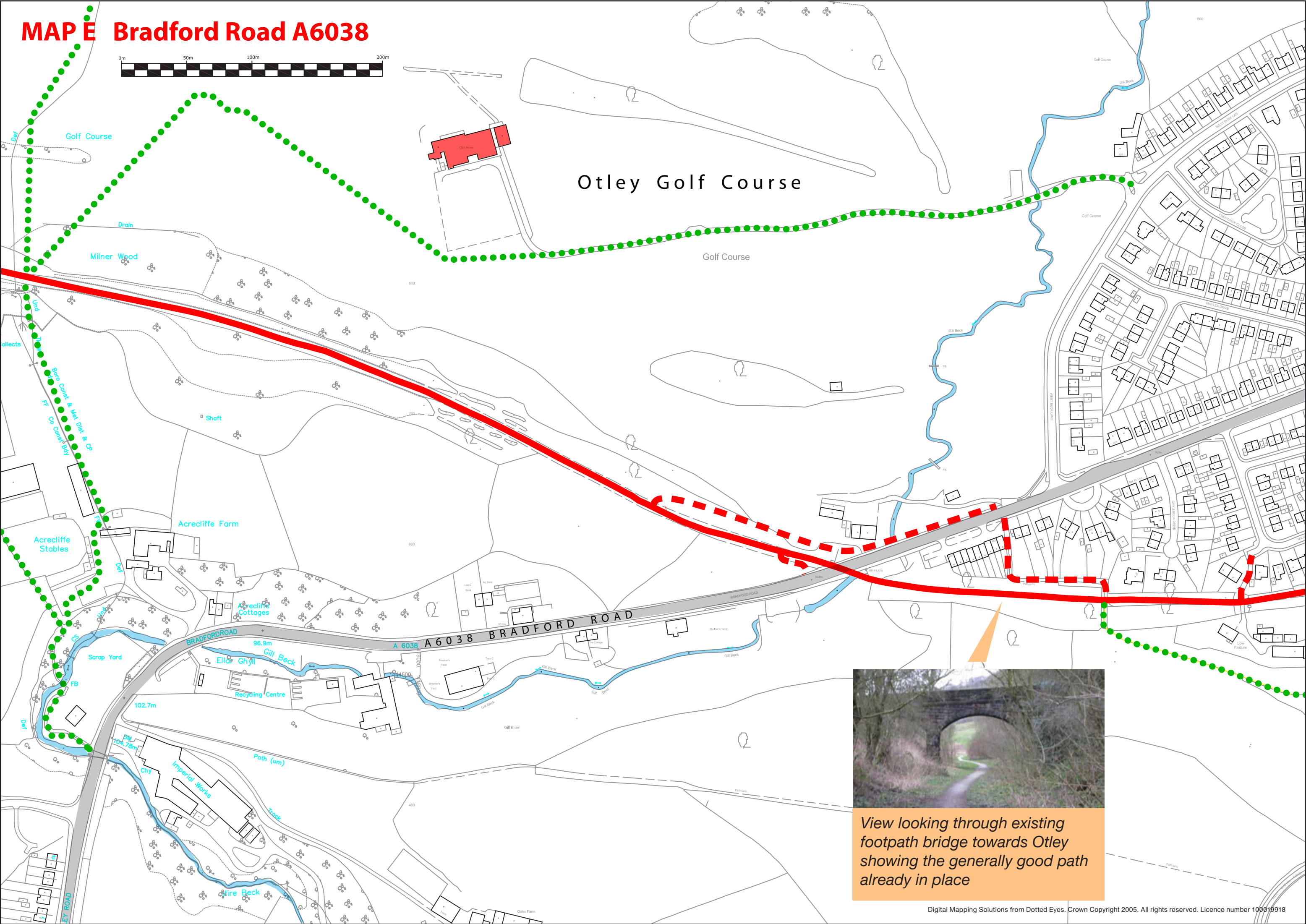
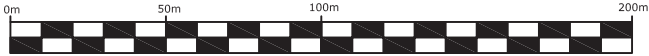
27m span steel warren truss bridge



MAP D Menston Station

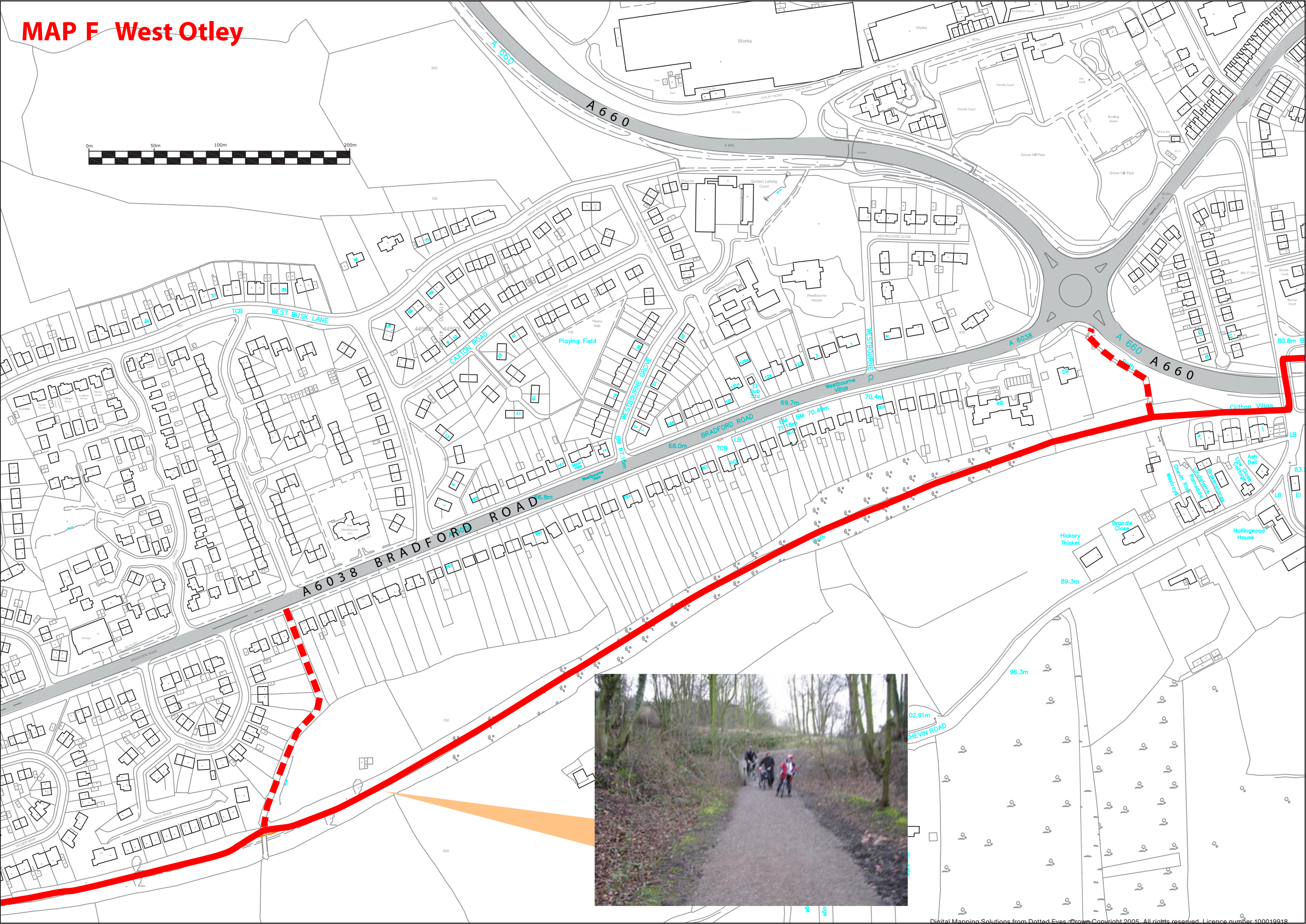
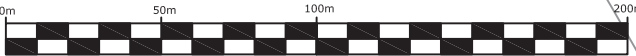


MAP E Bradford Road A6038



View looking through existing footpath bridge towards Otley showing the generally good path already in place

MAP F West Otley



MAP G Otley

**Otley
Town Centre**

All Saints Church

Waitrose

Sainsbury

Section 1

Section 2

Section 3

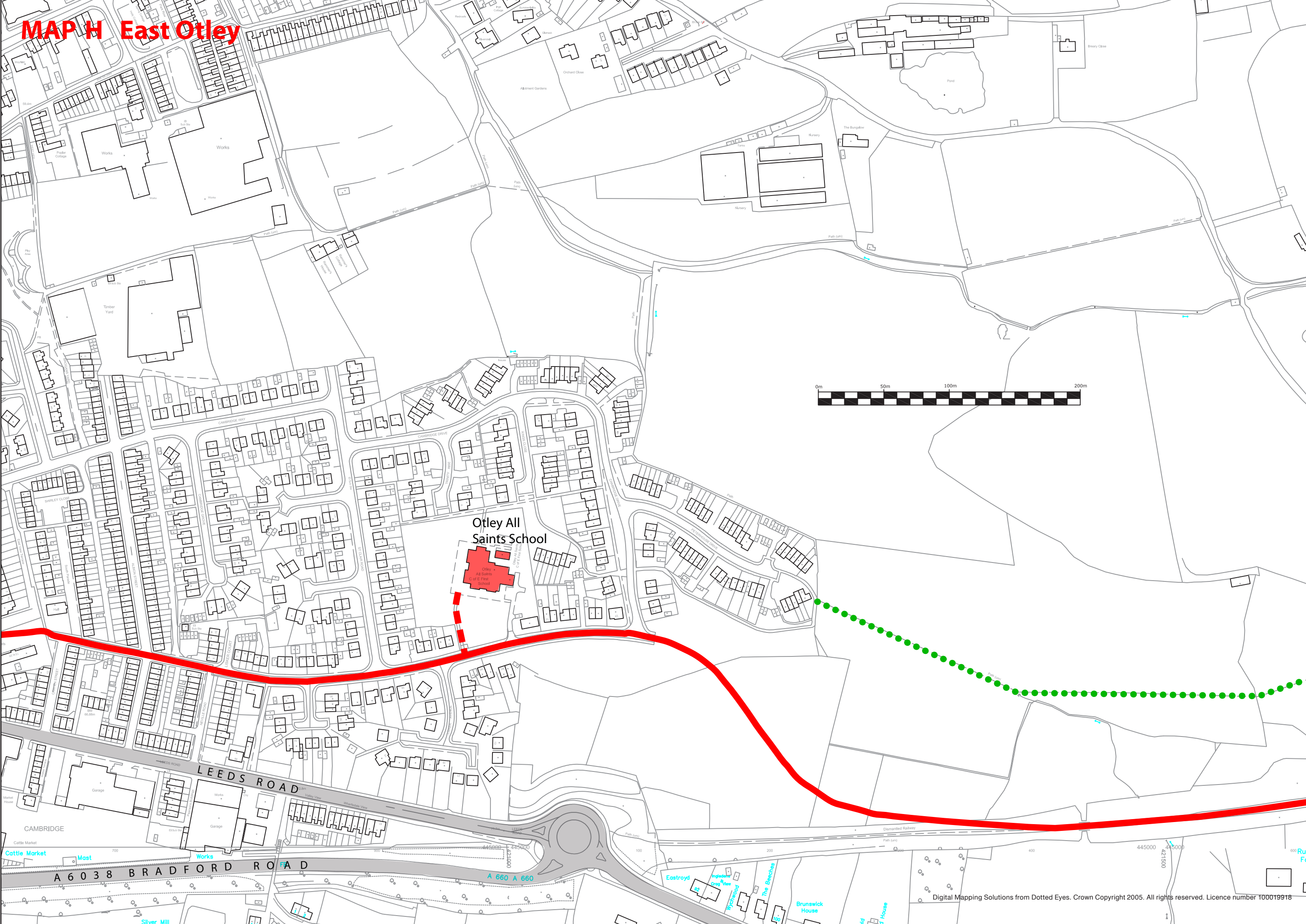
Section 4



View looking west in the original station area showing the extent of space available over most of the length

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MAP H East Otley



Otley All Saints School

Otley All Saints C of E First School

Cattle Market

Most

Works

A 6038 BRADFORD ROAD

Silver Mill

A 660 A 660

Eastroyd

Ingledene

Crag View

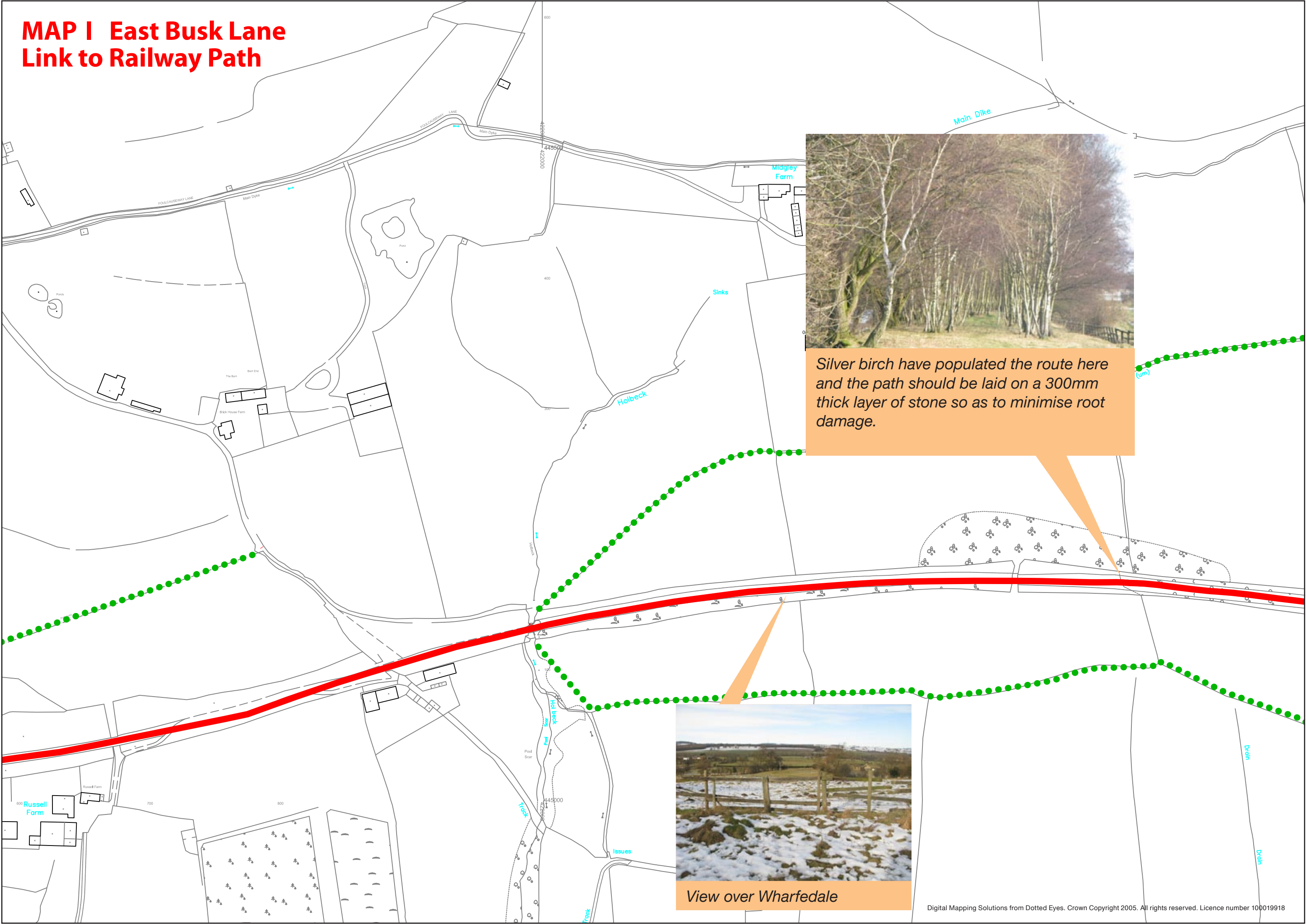
Wychwood

The Beeches

Brunswick House

St House

MAP I East Busk Lane Link to Railway Path

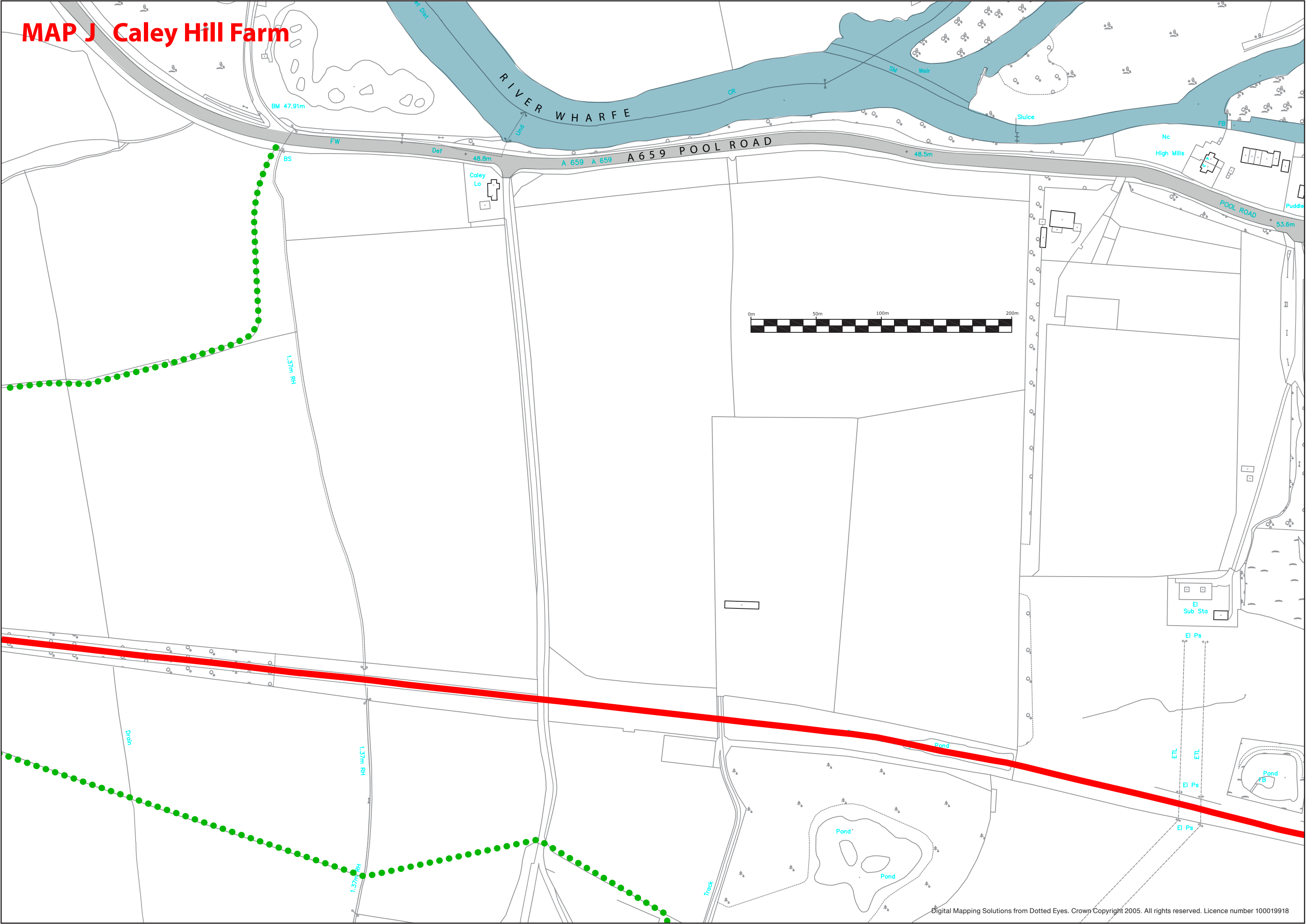


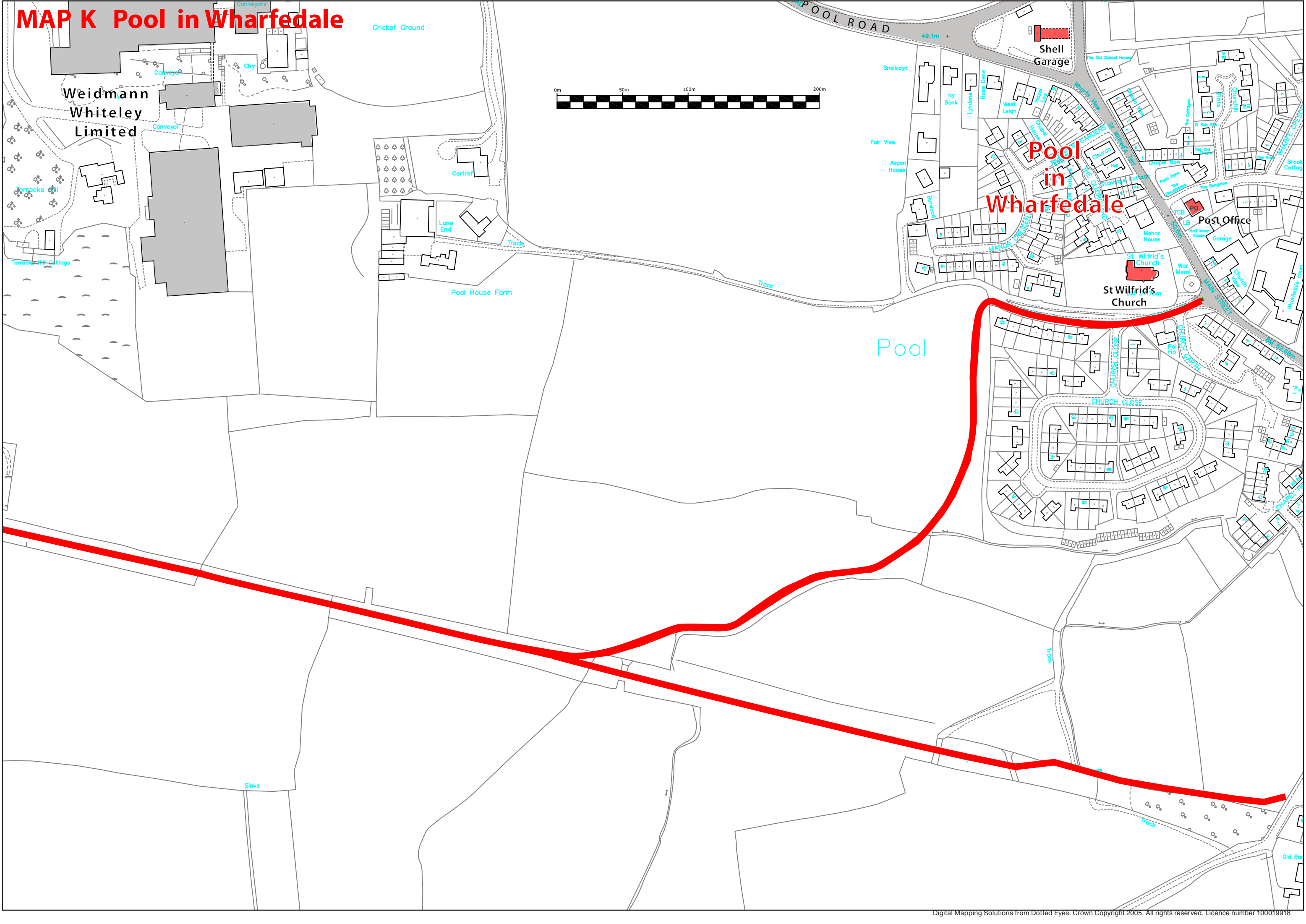
Silver birch have populated the route here and the path should be laid on a 300mm thick layer of stone so as to minimise root damage.



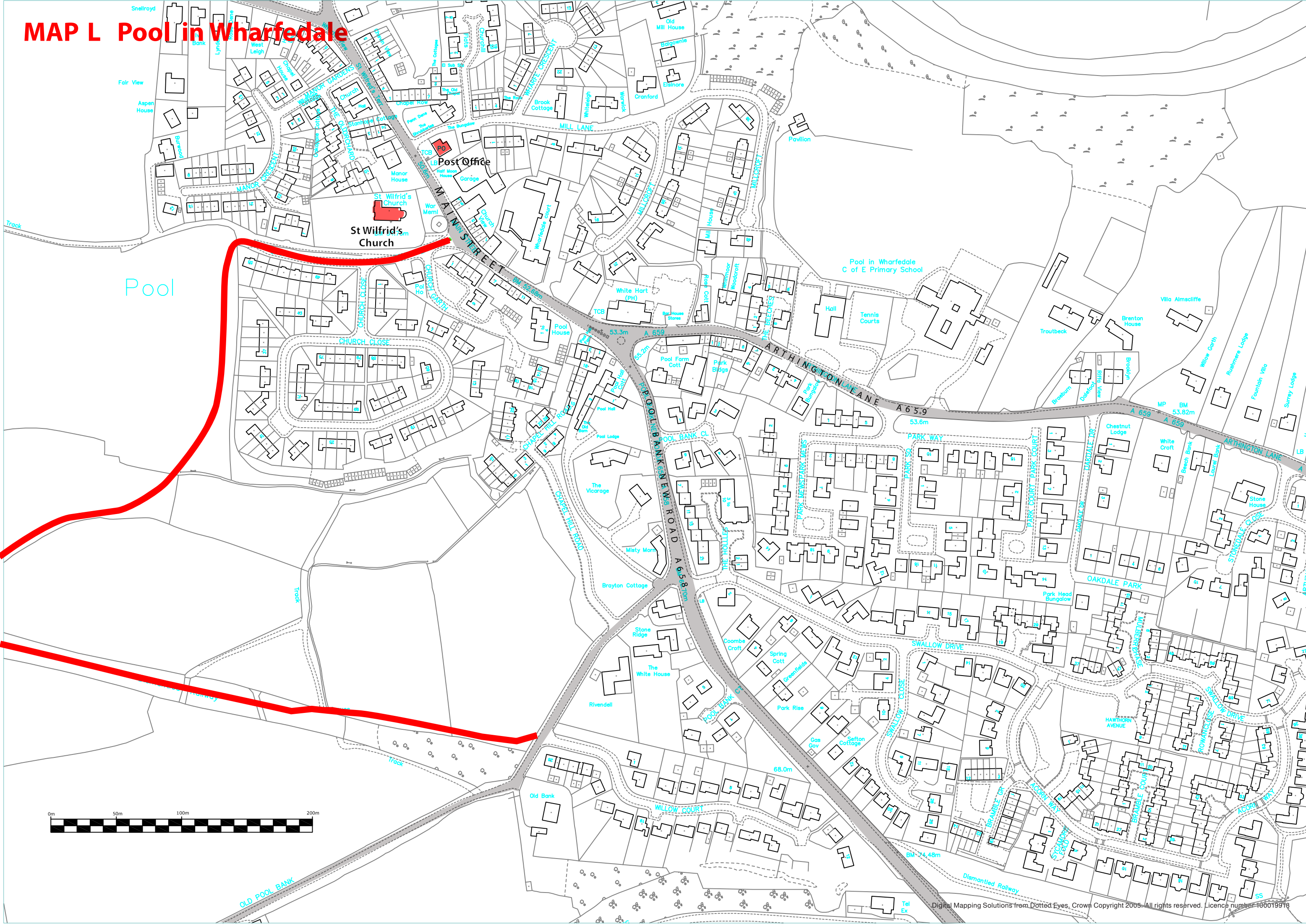
View over Wharfedale

MAP J Caley Hill Farm



[illegible][illegible]

MAP L Pool in Wharfedale



Pool in Wharfedale, Otley, Menston, Burley in Wharfedale **Cycleway Feasibility Study**

with links to Guiseley, Yeadon, Ilkley and Harewood House



River Wharfe Bridge, Pool-in-Wharfedale

DRAFT



**November 2009
Updated July 2010**

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Council Leader, Otley Town Council.

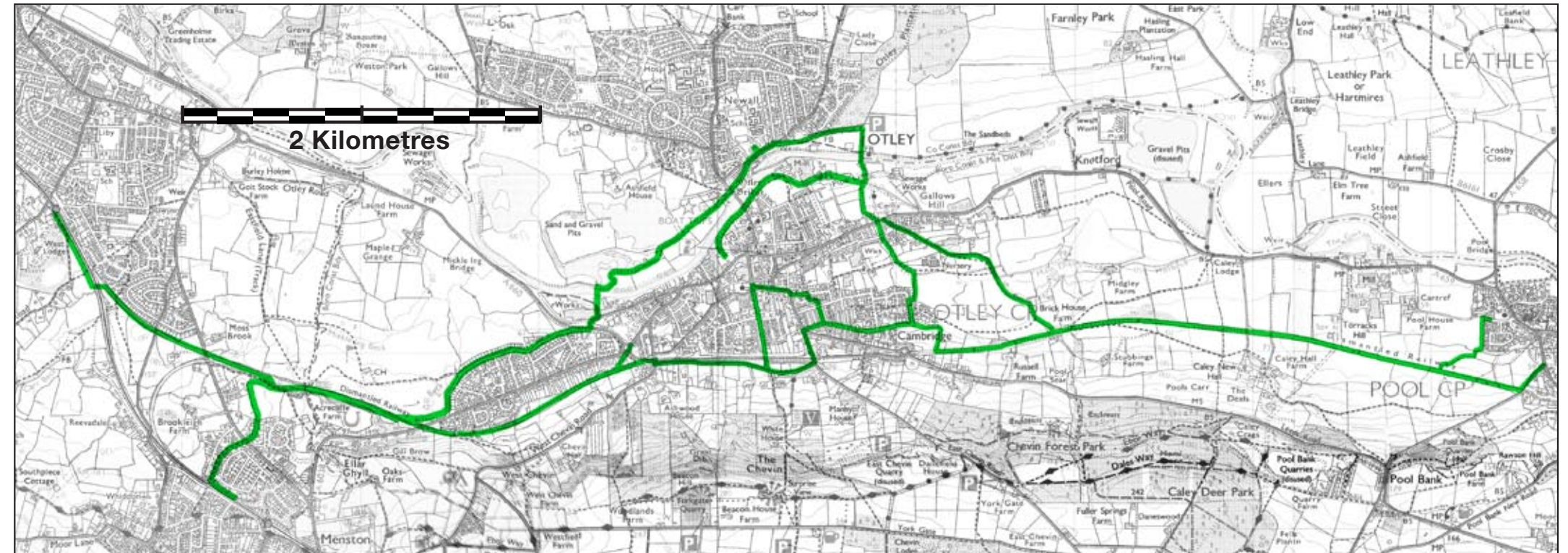
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3. Current obstacles to cycling and the railway path alternative
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5. Technical notes
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7. A possible programme
8. Conclusion

Appendix 1: A note on links to Harewood House, to Ilkley and to Yeadon and the Leeds and Liverpool Canal

Appendix 2: Land ownership records

Appendix 2: Previous documents and studies

This study was prepared by Mike Babbitt and John Grimshaw, with additional photography by Michael Bartholomew, typed by Rebecca White and the report designed by Paul Boston.



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Foreword

It is a pleasure to write the foreword for the Pool in Wharfedale in Wharfedale, Otley, Menston and Burley in Wharfedale Cycleway Feasibility Study.

The study is both timely and challenging.

It is timely because in the last two to three years the issues of healthy communities and global warming have truly come to the fore in people’s minds and in the media.

Otley Town Council has long been committed to projects which improve and enhance the traditional green issues, matters of health and physical wellbeing for Otley’s environment and population and we are pleased to back this project with its obvious benefits to both.

Cycling and walking are two of the most readily accessible means of maintaining one’s health and at the same time reducing one’s carbon footprint. Not only that, they can be done at any level, from the committed racing cyclist or long distance walker to the daily walk or cycle to the newsagent; both are important and contribute to the wellbeing of a community.

Sustrans have looked at cycleways around Otley, which are also suitable for walkers, and that link the communities of Otley, Pool in Wharfedale, Menston and Burley in Wharfedale, not only to each other, but also to the greater transport network.

The projected routes within this study will be readily accessible and easily negotiable so that few of us will have any excuse not to make full use of them.

It will be challenging because to build the routes will take a great deal of funding, time and commitment, and in that regard we are grateful to Leeds City Council for paying for this study to be done by Sustrans. All of the statutory and non-governmental organisations in our area will need to work and pull together to deliver these routes. We must hope they see it as their duty to deliver these improvements to our health and environment.

Sometimes it is hard to take a feasibility study and really have faith that it will be delivered. However, the late writer George Bernard Shaw had a message for us all when he wrote something which was often quoted by the late US Senator Robert Kennedy:

“You see the world as it is and ask why? I dream of the world as it could be and ask why not?”

Town Councillor James M Spencer, Council Leader, Otley Town Council.

1. Introduction

Otley and Pool in Wharfedale lie alongside the River Wharfe at the bottom of the steep escarpment of the Otley Chevin, which cuts them off from Leeds to the south and the Ilkley Railway which runs through Menston and Burley in Wharfedale. Although Otley town centre is relatively close to stations on this line (3 miles) the terrain coupled with the volume of traffic on the main roads makes cycling all but impossible. For Pool residents Otley is separated by another main road and in particular pupils to the Grammar School, again only three or so miles away, are cut off by traffic on this road.



The popular Bristol and Bath Path beside Avon Railway

The objective of this study is to set out the opportunities and details for a largely traffic free walking and cycling route along the remains of the former railway, to give a direct route to Menston and Burley in Wharfedale stations and to link Pool in Wharfedale to Otley. This study will show how this can be done. It will set out the necessary details, indicate the issues over land ownership and necessary negotiations, and provide an estimate of costs together with potential sources of funds.

These railway routes would be a wonderful asset for the area as well as transforming opportunities to make local journeys in a sustainable way. They will be suitable for walkers and cyclists and will provide a route through the countryside for the elderly, those with babies in push chairs and wheelchair users. Appropriate sections of route may also be suitable for equestrian use. The protection of these routes and their use as popular paths will ensure that the disused railway corridor is retained for future railway use, should this become possible. The double track formation could hypothetically be used for a path together with a single line railway.

In addition to the principal focus areas of Otley, Burley in Wharfedale, Menston and Pool in Wharfedale, further work has been done to expand the scope of the study to include suggestions for the best links to Guiseley, Yeadon, Ilkley and Harewood House. These can be seen in Appendix 1.

Schedule of distances along the proposed routes compared with those available on main roads		
Otley town centre to Menston Station	4.3km	3.4km via Bradford Road
Otley Town Centre to Burley in Wharfedale Station	4.5km	4.9km via A660 – Ilkley Road
Otley Town Centre to Pool in Wharfedale	4.7km	4.6km via A659 – Pool Road
Pool in Wharfedale to Prince Henry’s Grammar School	4.9km via White Bridge	5.3km via A659 – Pool Road

2. A discussion of possible Otley cycling schemes and route options

This study follows on from other work by Leeds City Council and Otley Town Council. This looked at a number of schemes within the town as well as the wider opportunities in the shape of the networking corridors. In this chapter we will briefly cover the most practical parts of these earlier studies, and consider any options which might exist of following the railway routes.

A note on a draft Otley Cycling Provision Study prepared 20th March 2007 by Mark Robinson of Leeds City Council.

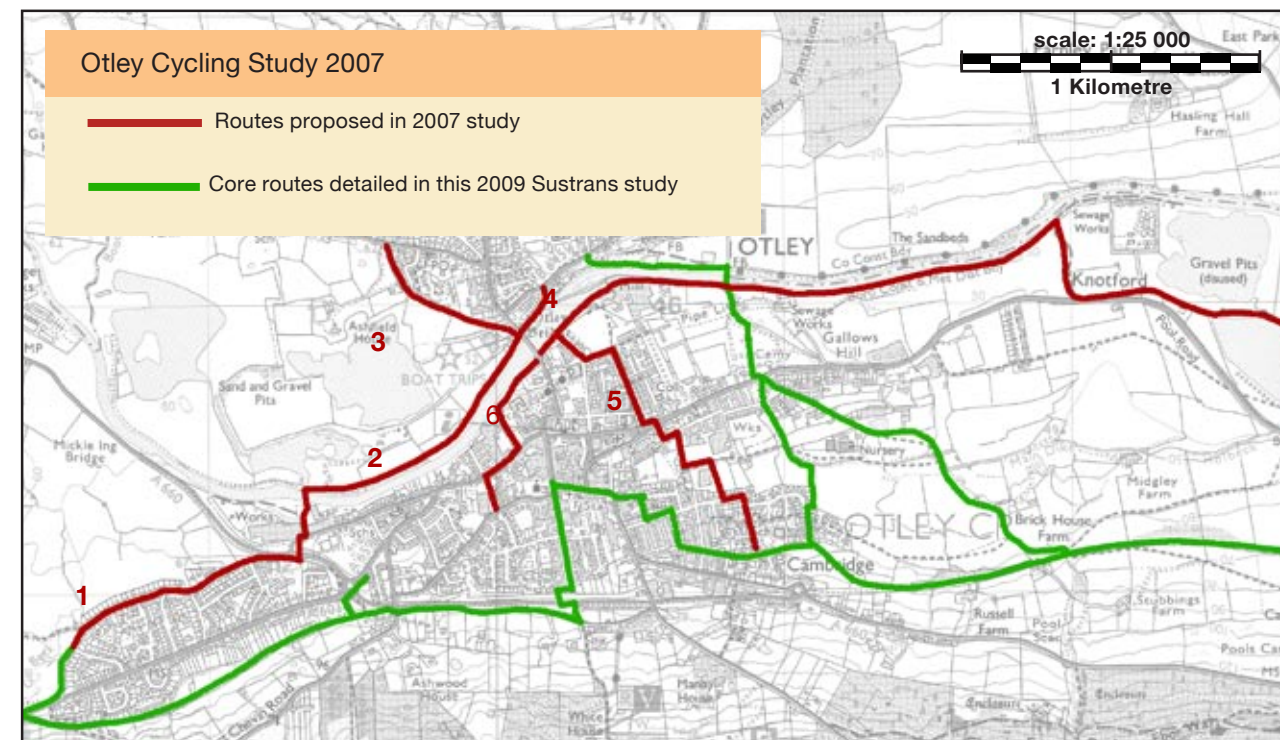
This study had a preliminary look at the opportunities for enhancing cycling and walking in Otley. As well as the core routes along the disused railways to Burley in Wharfedale, Menston and Pool in Wharfedale, which are the principle subject of the current Sustrans study, this earlier one covered a number of very useful routes and links. We have reviewed these and endorse the following, all highlighted on the summary map of Otley.

1. West Busk Lane via A660 Underpass to Ilkley Road

This is a very useful measure indeed and one already used by cyclists. Very little work is needed to formalise its use. We suggest that the existing path is wide enough for the fairly light shared use expected, the subway is excellently arranged with good sight lines, but a flush kerb is needed to connect to the Ilkley Road.

2. Western River Bank and Planned Otley Mill Bridge

Otley Town Council's Riverside Vision strongly supports the development of the riverside and a new bridge crossing at Otley Mills. When this goes ahead then it can be arranged so as to provide a good link over Ilkley Road from the West Busk Lane subway, and thence via this bridge over the River Wharfe with a new riverside route creating an almost perfect route to Prince Henry's Grammar School for those pupils living to the west of Otley. The riverside route along the north bank of the riverside should be arranged as a promenade so that it becomes a real asset for the town.



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Bridge Avenue - good width and surface but No Cycling and heavily used by school pedestrian traffic

At Otley Bridge itself a good crossing of the main road is needed to reach Bridge Avenue for this route to the Grammar School. This may take the form of a zebra crossing to give a good priority for pedestrians and could include a de-restriction for cyclists. Alternatively a Toucan crossing could be installed.



An example from Hereford of a cycling permitted zebra crossing

3. Green Lane and Church Lane links

Particularly if the Western Riverbank Promenade goes in, then this link to the north western part of the town, and the back road to Ilkley, will be most valuable. The narrower section of the existing footpath should be widened as shown in the sketches, and the route continued to join the Western Riverside itself.



View to SE narrow width with fast flowing drainage channel. Between 0.5-1.0m extra width available to fence/hedge line for most of this section

4. Garnetts Mill and Riverside Path to Knotford

The redevelopment here will give the opportunity for another good riverside path and a local circuit via the White Bridge. Although an access road is planned via Gallows Hill to the Pool Road this does not make for as good a connection onto the southeast part of the town as does a route through the cemetery as proposed in the main part of the Sustrans study.

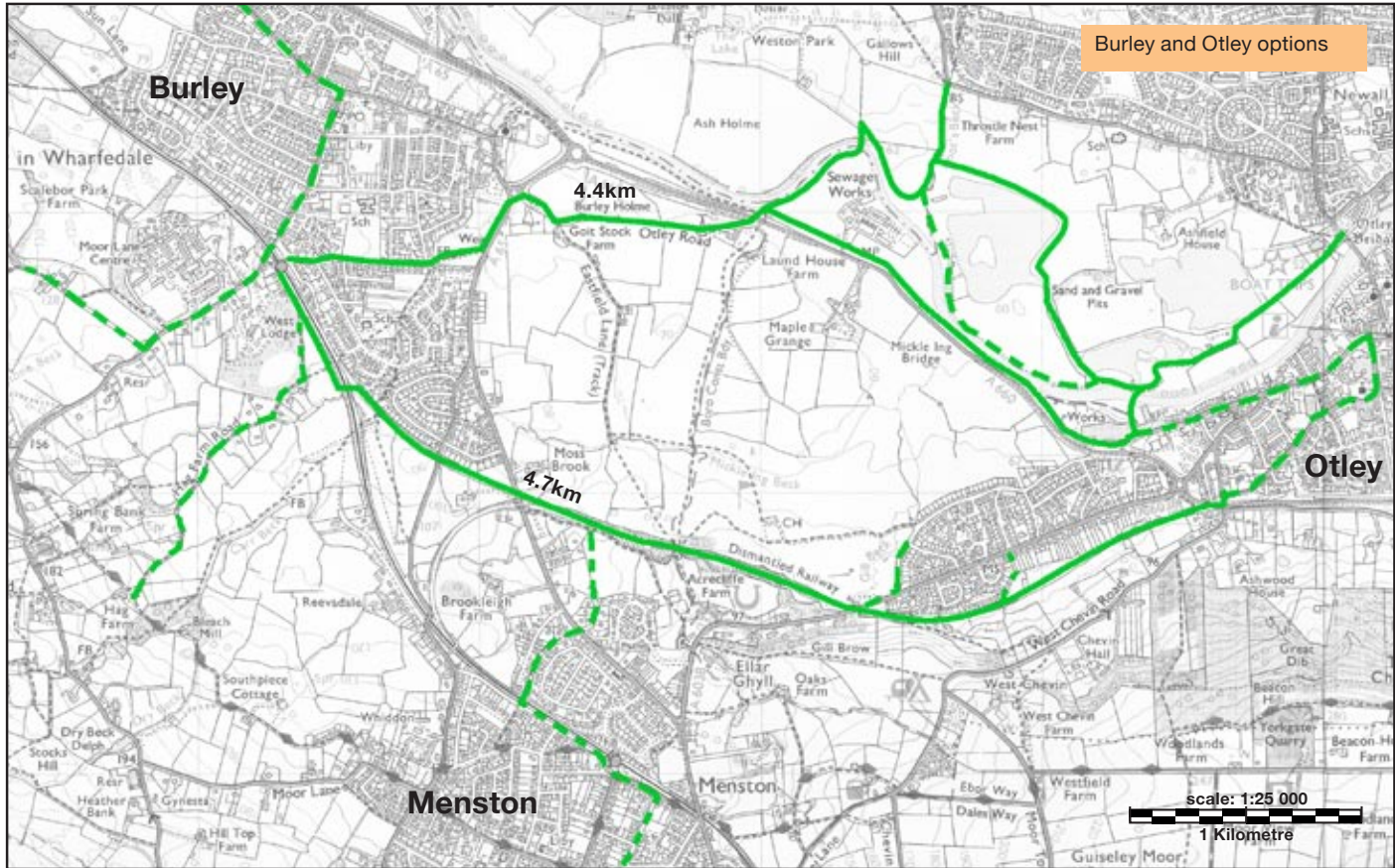
This development should also be a catalyst for continuing the riverside route as far as Knotford and the large gravel pit lakes there. This would be another real asset for the town, and would come within striking distance of a possible main road route to Pool in Wharfedale.

5. Cross Green and Cambridge Walk

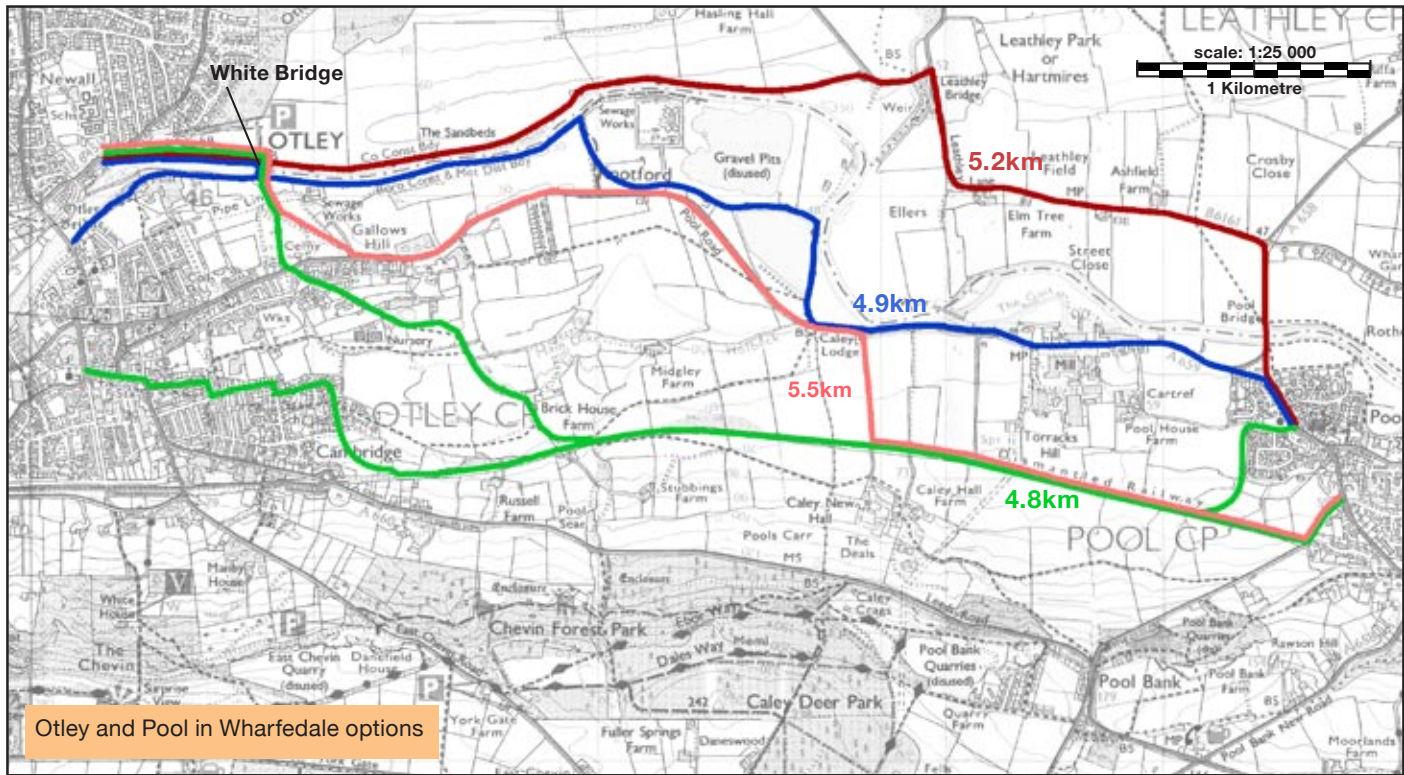
The 2007 study proposed this useful link which would be well worthwhile doing, particularly the footway widening between Crow Lane and Albion Street.

6. Ashford Works

Although only a short length of route is planned here, it would be useful in connecting the southern part of the town to Otley Bridge and thence to the Grammar School. This route will require the shared use of the existing riverside path behind the Primary School, but one might hope that the public will support this in view of all the other riverside additions planned in the ongoing development of Otley.



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Options to Burley, Menston and Pool in Wharfedale

Burley in Wharfedale Station is 20m lower than Menston Station and a route there from Otley might be even more of a prize than to the slightly nearer Menston. The only current alternative to the railway route is the main Ilkley Road A660. The two routes are shown on the map. In addition there might be merit in considering bridging the Wharfe just upstream of the sewage works and weaving a countryside path around the worked out gravel pits. This would be particularly advantageous for the Western part of Otley.

The distances for each route are similar, about 4.5km (3 miles), but the A660 route requires the least construction, just 1.4km of new path. However even if this new path was set well back from the road, in a greenway of its own, it would still be close to the traffic and rather less attractive than the railway alignment. Despite this, we think that one of these A660/ riverside options would be the best for the Otley – Burley link, but there is a need to link Menston as well. The railway path is the only option which has the capability of linking Otley to both Menston and Burley Stations, and also of linking together all three communities for local trips and meetings. So the railway path is the recommended scheme.

From Pool in Wharfedale to Otley, there are four possible routes. On the north side of the river, Leathley Lane links to the riverside track leading directly to the school, and is perhaps surprisingly only a little longer (5.2km) than following the Pool Road to Caley Lodge and then the riverside from Knotford (4.9km). However new construction would be needed from Pool Bridge to the crossroads (and crossing Pool Bridge is tricky), although this would also give access to Castley lane and a network of minor roads for leisure cycling. The use of the eastern end of Pool Road is less desirable due to the possible conflict with access to the large factories and fast moving traffic. The railway route is slightly shorter again and gives better connections to most of the population of Pool in Wharfedale who would otherwise have to use the main road through the village to reach the Pool Road. That road has a good footway for some of its length which could form the basis of a traffic free route but there are severe width restrictions on some parts of the road.

The combined option of using the railway, then down to Pool Road to Gallows Hill is the longest route and would not provide a continuous experience due to the proximity of the road for a significant section.

The railway path option is the more attractive as a real asset for the communities, as well as acting as a safe and direct everyday route from Pool in Wharfedale to Otley. It happens to be the shortest route available, provides the most convenient access opportunities from Pool in Wharfedale to both the school and the centre of Otley and would potentially be the most cost effective to construct due to the existence of the railway formation.

3. Current obstacles to cycling, solutions and anticipated use

Once out of the floor of the valley the terrain is very difficult for cycling, a barrier which is magnified by the volume of traffic on the main roads in the area.

Average daily flows on selected roads		
Road	Route	Vehicles per day
Bradford Road	Otley to Menston	10,700
Burley Road	Menston to Burley in Wharfedale	11,500
A660 Ilkley Road	Otley to Burley in Wharfedale	18,400
Pool Road	Otley to Pool in Wharfedale	10,100
Pool Bank New Road (A658)	Pool in Wharfedale to Leeds Road	17,800
Leeds Road	Otley to Bramhope	15,200

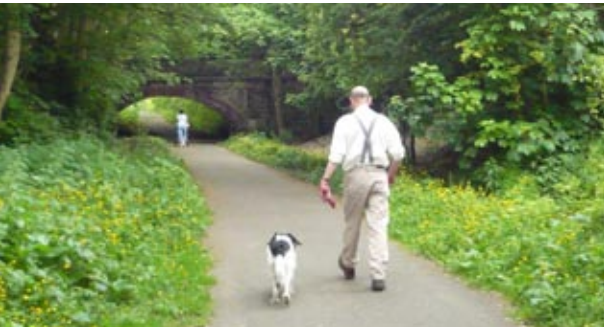
All these flows are such as to suggest the need for a traffic free route separated from the highway in order to give confidence to novice cyclists of every age.

The central purpose of a proposed railway path is to create such an attractive and convenient route that it will become popular for everyday journeys to the stations, to school and recreational journeys of every kind, including visiting friends, casual walks or rides in the locality of the towns and part of longer journeys either taking the train to Ilkley and continuing to explore further up Wharfedale or picking up minor roads to the north of Leeds. The encouragement of local trips, especially ones on foot and cycle, is an essential ingredient of any policy to encourage more sustainable transport by reducing the distance we have to drive, and for bringing about a more active, fitter and less obese population.

These ambitions are coming more and more to the fore as we appreciate the extent to which our dependence on the motor car has brought about a sedentary society and one which creates high levels of CO₂ and other climate changing emissions.

The population of the various towns are as follows:

Burley in Wharfedale	6400
Menston	5,500
Otley	14,100
Pool in Wharfedale	3000



Yeadon to Guiseley railway path

Ordinarily a good railway path in this sort of location would be expected to carry between 20-40,000 trips per kilometre per year, or say 200,000 trips per year between Burley in Wharfedale and Pool in Wharfedale. About half of these would be pedestrians. In this particular case the route is likely to appeal to those who travel to Leeds to work and perhaps anything upwards of 500 people might find it attractive to cycle to Menston Station each day rather than drive as at present. In addition some 50 pupils a day travel from Pool to the Grammar School, almost all of whom would find the railway path more attractive with the consequent saving in school transport.



Pool bridge

4. Description of the Railway Routes

The preferred proposal follows the disused railway from its junction with the live line in Burley in Wharfedale through to Otley and Pool. The railway formation is largely intact. At the Burley in Wharfedale end where the original junction stops short of the station itself good links need to be made to Heather Rise on one side and Hag Farm Road on the other in order to reach the station. A bridge is missing at Menston Old Lane and more importantly across Burley Road (A65) and then again across the Bradford Road (A6038). In between these two gaps there is a magnificent section of wooded cutting and embankment which makes for a memorable destination in its own right. In Otley itself the railway has been turned into a formal path as far as the Leeds Road where a critical link is needed to lead through to Birdcage Walk for the existing bridge over the main road leading to Kirkgate and the town centre. Through Otley itself the remains of the railway lie very close to the new main road which is partly built along the railways alignment and is difficult to reach from either end. East of Otley the railway is again largely intact although some is privately owned but at the Pool in Wharfedale end a new link around the field edge will be needed if it is to be of any benefit for local residents in Pool in Wharfedale who need a connection to Church Lane, as well as linking through to Pool Bank for the eastern part of the village.

Three other significant connections are required, the first to Menston Station which is best done via a new field edge path from the railway line to Beech Close whence Carlayne Lane provides an attractive route up to the railway, Westbourne Drive, Fairfax Avenue and the station. Another important link, but this time coming from Pool in Wharfedale, leads down to East Busk Lane and thence via the cemetery to the White Bridge for a route through the park to the Grammar School. Lastly we need to devise a route up the hill to cross the Leeds Road to reach Otley Chevin with its miles of forest paths and its links to Leeds Local Cycle Route via York Gate.

The detailed maps and proposals which follow set out to describe the route and the works in sufficient detail to allow the project to progress.

The recently opened railway path between Yeadon and Guiseley is proving very popular and may be taken as a good example of the sort of railway path project we envisage here.



Yeadon to Guiseley railway path



View of existing path close to former site of Otley Station

Map 1. Burley in Wharfedale

- Existing link from Hag Farm Road to north bound platform at Burley in Wharfedale station.
- A new path along the edge of the field would provide for the most direct and attractive route to the station, this connects under the railway via an existing bridge under the railway near the original junction.
- The path here is quite steep and it would need to be carefully graded, with a ramp as shown in the sketch to insure that the route was useable by everyone.



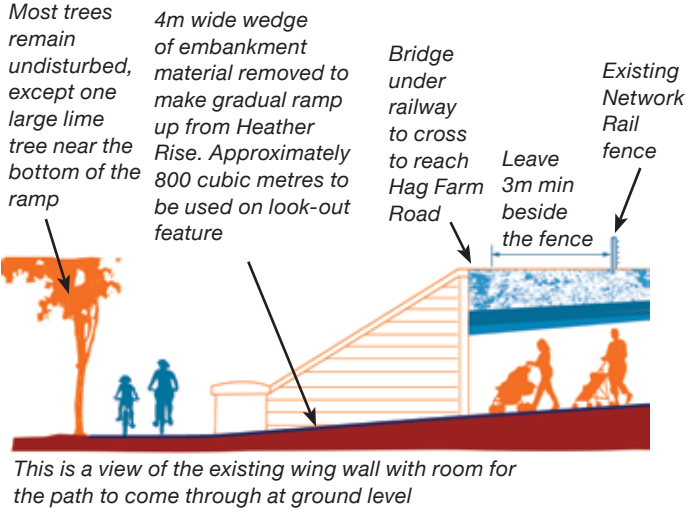
Photograph of the subway under the railway showing how the path would loop around in order to give an easy gradient and how it would be fenced form the field to deal with livestock

- The link to Heather Rise needs improving especially as it provides a direct connection to Burley and Woodhead Primary School, who will find the railway path a considerable resource. This will also enable an additional link to be made on Prospect Road and down the side of the playing field to link directly to Burley Oaks Primary School.



Link to Heather Rise

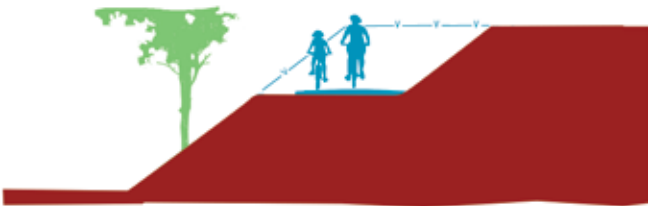
- A 1:20 ramp should be achievable here by excavation of the former railway embankment. Surplus material can be placed in the cleft between the two railway embankments to create a feature, possibly even one explored by the primary school children.



This is a view of the existing wing wall with room for the path to come through at ground level



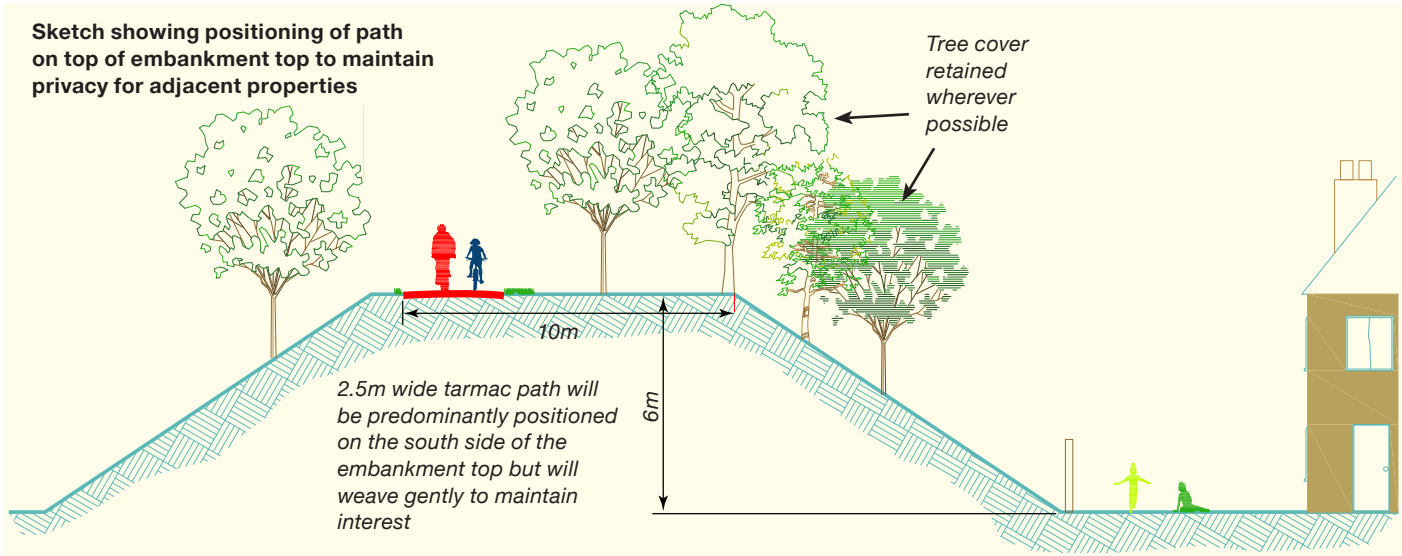
The whole bank then has a wedge 4m wide tapering from full height here to nothing 100m down the line. Here the path position is about 20m from the wall



Path position about 60m from the wall
 Heather Rise Ramp: Views looking towards Otley



View looking up through the railway subway. A new surface is required and the field gate at the end could be removed if the main path was fenced



Typical section round point 8.



Typical view of the railway path just east of the subway. The Network Rail fence on the right could possibly be rerouted parallel to the Ilkley line to give a little more room to make the ramp down to Burley at that point

- The link to Holme Park is of considerable importance because it in turn connects through to a footpath which winds its way northwards past a number of cul-de-sacs and parallel to Woodhead Brook, thereby giving access to much of the town. In order to achieve a good connection a long ramp needs to be built, as shown in the sketch, note that by dropping the level of the main railway path it is possible to reduce the length of these connecting ramps, to replace the existing steep scramble up the side of the embankment, currently followed by the existing public footpath.
- This footpath goes past the interesting wild area between the two railway embankments which could be developed as a local nature park with an informal walk winding around it.
- Over this section a path should be set well to the south of the embankment in order to minimise overlooking of properties along Holme Grove.

MAP 1 Burley-in-Wharfedale



View towards Burley station showing the well used field path leading to the gate onto the lane at the end. The new path would run parallel with the railway fence

Page 6 opposite

Map 2. Bradford Road A65

1. Menston Old Lane Bridge is missing but its masonry abutments still remain and it would be a relatively simple matter to install a new bridge span, as shown in map sketch opposite, this would be definitely worthwhile to enhance the continuity of the route.

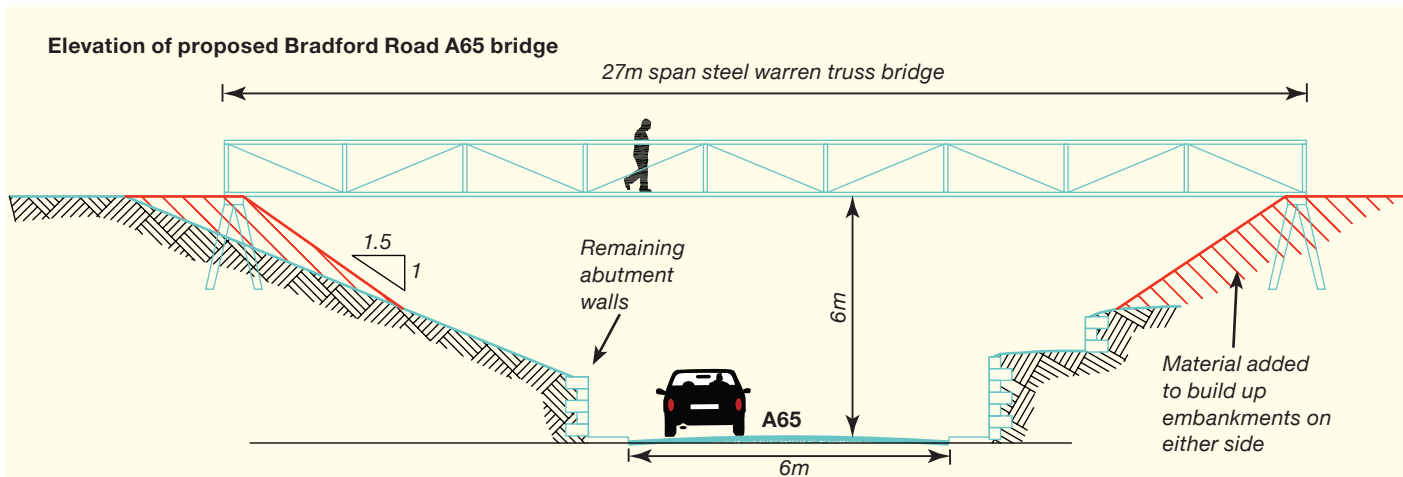


Menston Old Lane showing the substantial abutments left from the former bridge

2. A new ramp needs to be made onto the lane itself. The best of a number of options is on the south east side in order to minimise the length of ramp (Menston Old Lane is rising) again reducing the level of the main path by a metre or so in turn reduces the length of ramp required.
3. Continue along the south side of the embankment in order to minimise any over looking of Endor Crescent. At the same time clear vegetation on the south side of the railway embankment in order to let sun in and give views over the farmlands.
4. Discuss with the local farmer the most satisfactory details of this agricultural crossing and concrete the passage itself to eliminate damage from farm machinery.
5. It is essential to construct a bridge over the Bradford Road as this is heavily trafficked, straight and fast. The necessary 6m clearance can readily be achieved by constructing new earthworks behind the remains of the masonry abutment walls as shown in the sketch.



The remaining abutments of the Bradford Road Bridge which would form the basis of an embankment leading up to a new bridge crossing. The gates on the right would be the location of the works access



6. A works access road should be constructed here via the existing gate.
7. The embankment works are quite complex here due to the branch line leading off to Menston Station (note we did investigate the possible use of this line but concluded that it was impractical and anyway would form a rather circuitous route to Menston Station). The extent of these embankments gives the opportunity for creating quite a memorable space and we consider it best that the route climbs gradually to the upper level in order to make a better connection to Beech Close and Menston Station. The whole of this area is most attractive and there is much to be said for a lower level walkway to allow people to treat the whole of this area as a linear park.
8. The key link to Menston needs to be negotiated along this field boundary to create a relatively level route through to the town. Note that this would be very much more useful than the existing public footpath which goes past Acrecliffe Farm as this only leads onto the busy main road. Therefore we recommend that this public footpath is shifted to the farm boundary which may be advantageous to the local landowners.
9. Milner Wood is a fascinating section of this railway route. The existing path around the top of the former cutting or quarry should be retained as part of a circular walk and brought down to the railway at the City Council boundary.
10. This City Council boundary is a significant gathering point of local paths. No fewer than three fan out northwards across the golf course and we suggest that a very definite feature is made of this crossing point with an ample seat and vegetation clearance to give extensive views northwards over the golf course and Wharfedale beyond.



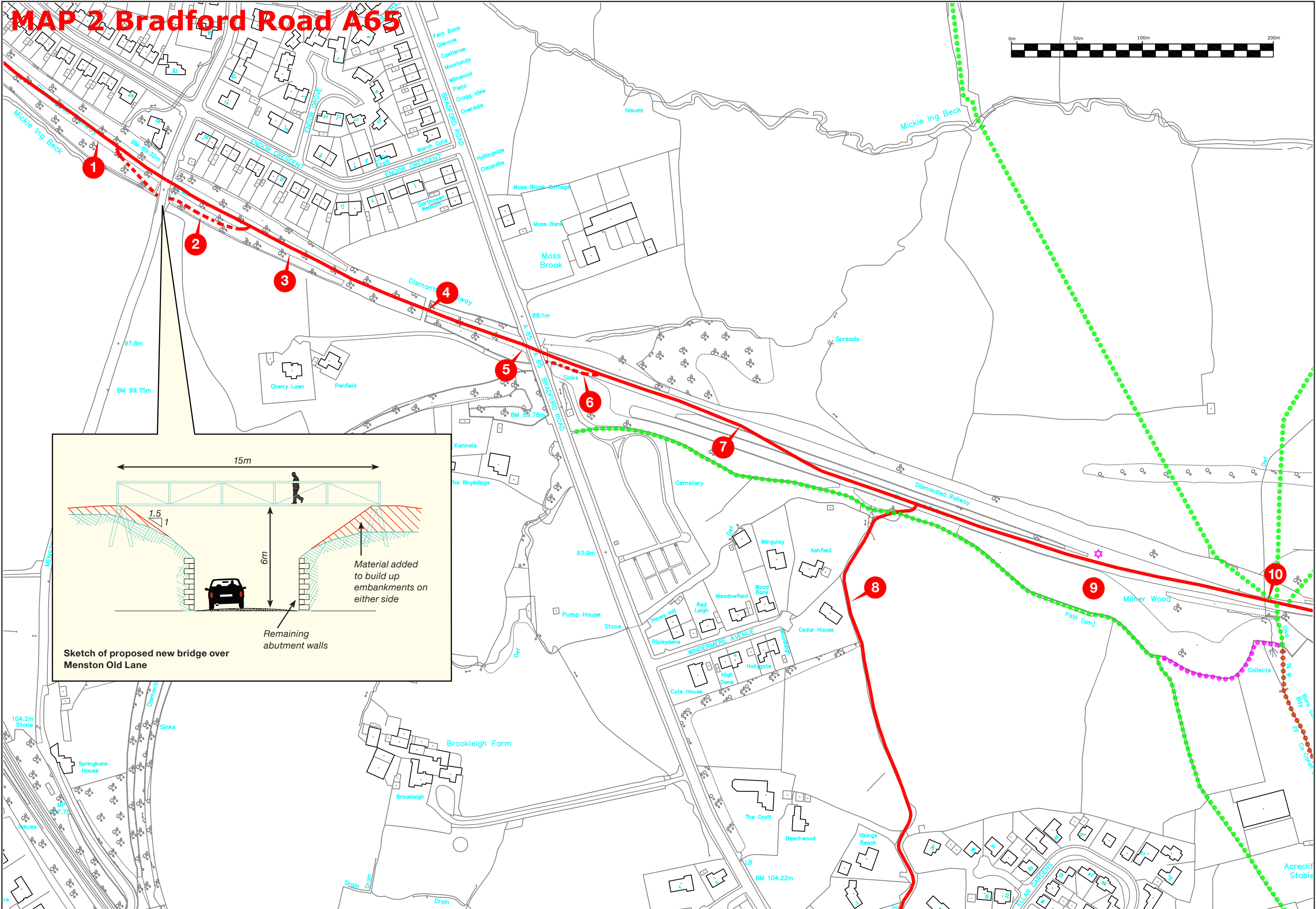
Example of seat from the Pheonix Trail in Buckinghamshire



View to North at the end of Beech Close the path would follow the edge of the farmer's field for approximately 250m and then lead up on to the core route on the railway path. The path would be fenced as necessary to maintain livestock

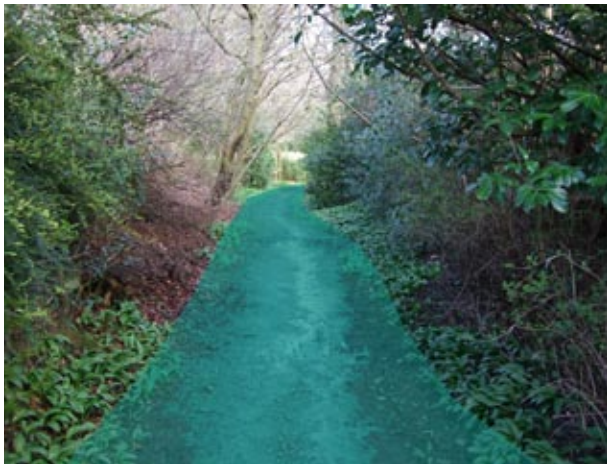
MAP 2 Bradford Road A65

Page 8 opposite



Map 3. Menston Station

1. The key link to the station follows Beech Close to cross Burley Road and link to Carlrayne Lane. Burley Road itself requires a toucan crossing to give a degree of priority to pedestrians and cyclists.
2. The lane is an attractive feature running up the back of Moorland Crescent and Halstead Drive. It retains its rural feel and has adequate width to be resurfaced as a shared use route.



Carlrayne Lane is 3m wide along its length and a 2.5m wide surfaced path is achievable with sympathetic vegetation clearance in agreement with neighbouring landowners

3. The link to Moorland Crescent, and indeed Halstead Drive, needs to be formalised.

4. Some work is needed to ease the gradient of the existing bridge over the mainline railway and its approaches. On the southern side, there is ample space to create a ramped approach in the wide verge of Westbourne Drive which leads on to Menston Station.



View of Westbourne Drive Bridge over the Ilkley line. It is envisaged that the rather steep ramp in the foreground could be augmented by an easier gradient going towards the station to the right



The open clearing from the rail bridge down to Carlrayne Lane can be used to curve the tarmac path around the ease the gradient

5. Carlrayne Lane continues southwards and it could be rebuilt as far as its connection to Victoria Avenue and Fairfax Avenue in order to give access to the western part of Menston.

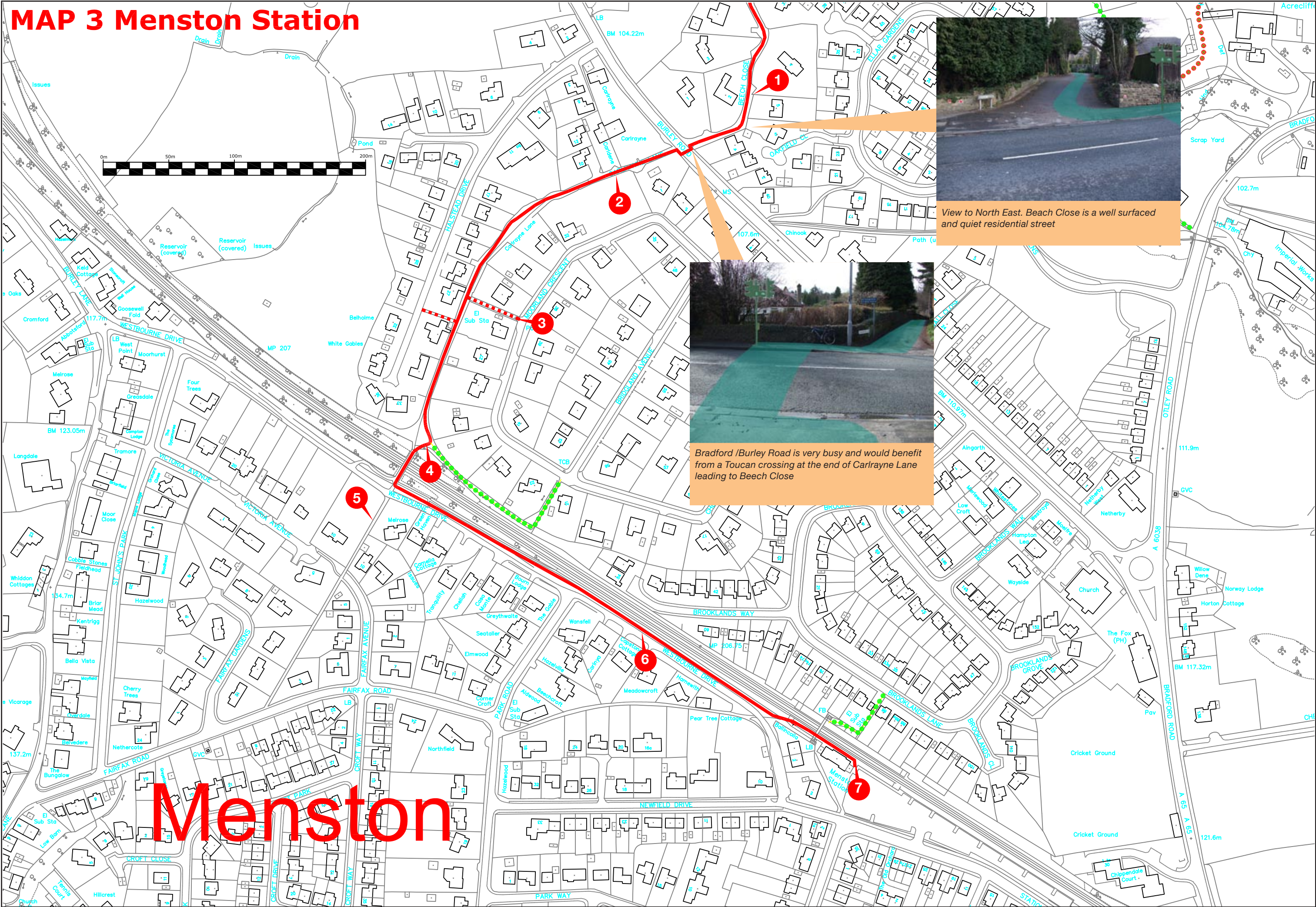
6. Westbourne Drive is unadopted and in poor repair. The 2.5m wide tarmac path could be situated within 3m verge as shown. Alternatively the whole road width could be made up to a good standard.



Route running beside Westbourne Drive

7. The final link onto Menston Station platform can best be achieved by a short ramp at the end of the platform in order to avoid the up and down steps near the station bridge. Extensive cycle parking should be provided on the south side of the station so as to avoid the need for the public to take their bicycles over the footbridge.

MAP 3 Menston Station



Map 4. Bradford Road A6038

- 1. This City Council boundary is a significant gathering point of local paths. No less than three fan out northwards across the golf course and we suggest that a very definite feature is made of this crossing point with an ample seat and vegetation clearance to give extensive views northwards over the golf course and Wharfedale beyond.
- 2. The path should be built on a low causeway through this wooded cutting so as to ensure that it is dry at all times (see Fig 4).



The wooded cutting at point 2 in its current state

- 3. The original formation has been modified here with fill but the railway still has a magnificent high level view to the north.



Above the golf course. The land to the right is fill from the tip adjacent to the main road



View looking towards Burley beside the landfill site and above the golf course. The ramp down to West Busk Lane would start just to the right of this picture

- 4. A most important link needs to be constructed along the north side of the Bradford Road as far as West Busk Lane. This will rely on an agreement with the golf course because the long ramp needs to lead down to behind their maintenance garages (see figs. 1 and 2) and then across Gill Beck on a new foot and cycle bridge (see fig. 3) to then reach West Busk Lane. This gives access to the route to all the residents on the north of the Bradford Road without their having to cross the busy main road.
- 5. The third of the bridges between Burley in Wharfedale and Otley is needed here to cross this busy main road. The original railway bridge had two spans and its robust central pier remains. For a new foot and cycle bridge, a two span structure could be used with the road span shifted to the northern tip of the western abutment to necessitate a shorter span whilst keeping a 6m road clearance. The second span crosses the deep basin of where Gill Beck passes under the main road. The beck would be an intriguing feature for path users, but one that is invisible from the road. Alternatively use a single span of both the road and the beck at approximately 39m long.

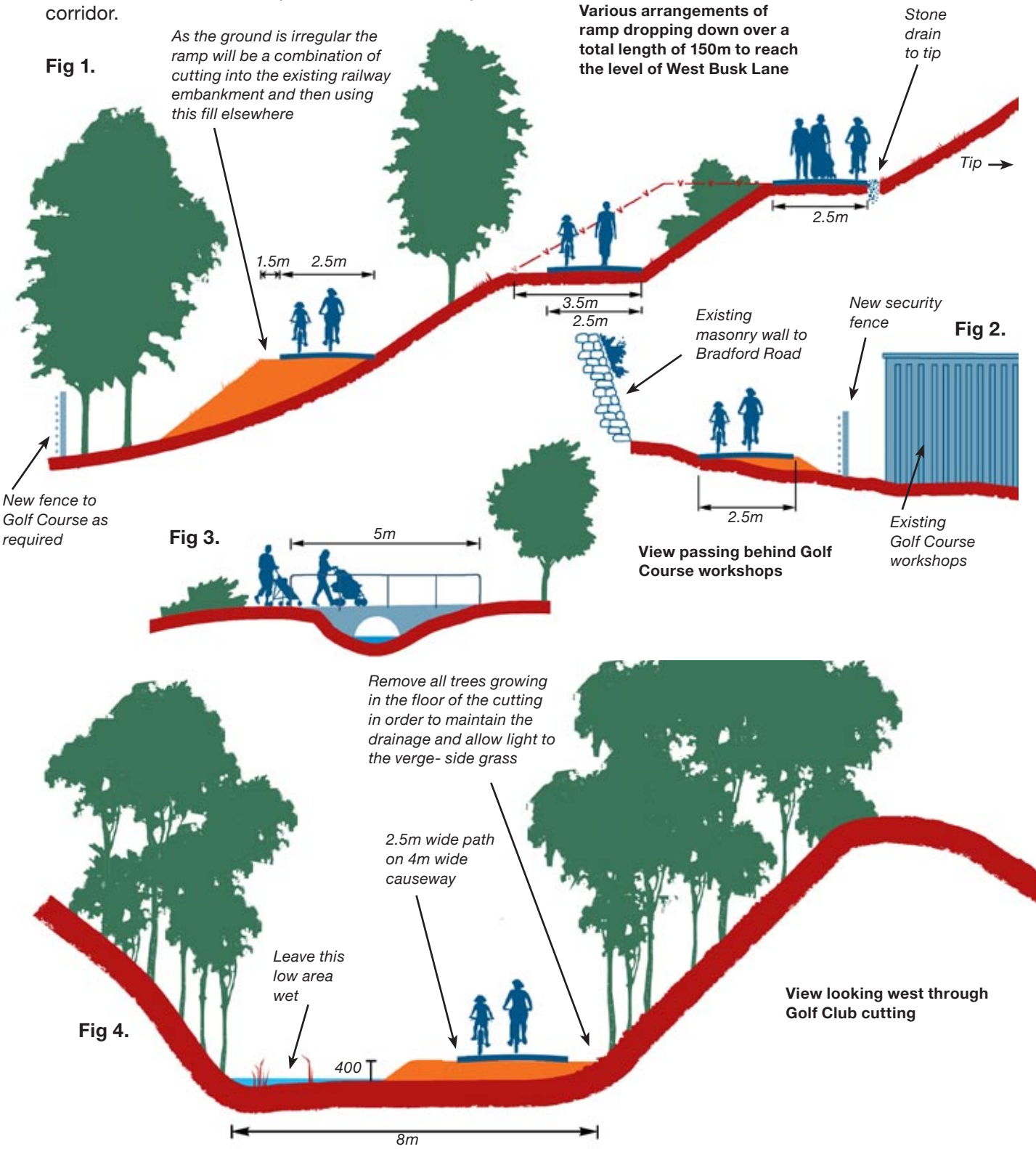


The former railway bridge abutments are still present and appear in good condition

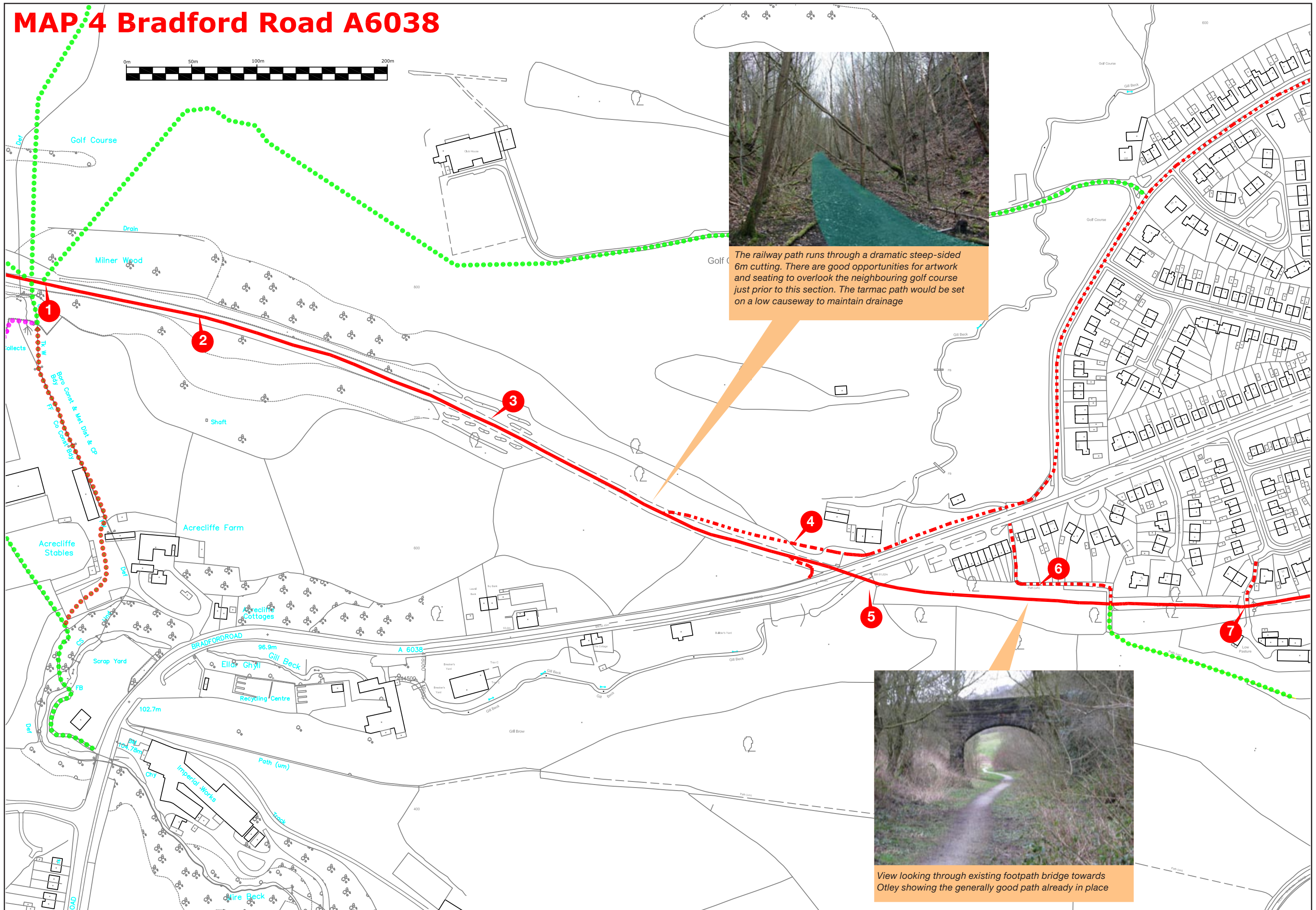
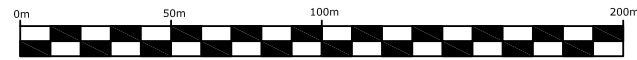


How a new single span foot and cycle bridge over Bradford Road would look

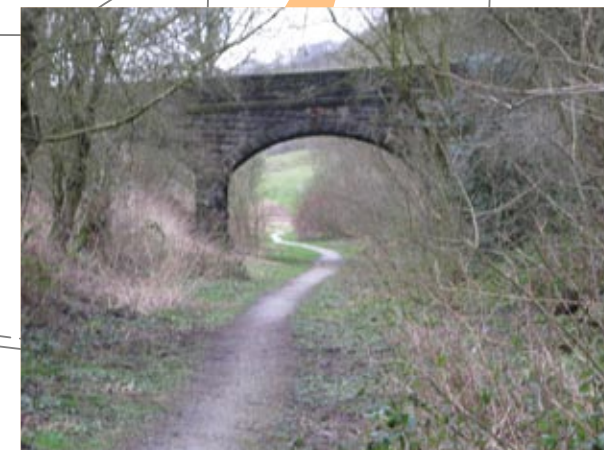
- 6. This existing access lane serves very few people and can probably be left in its current stony state.
- 7. The level crossing here gives a good access to Milne Bank and makes a convenient link for people in this area. From here on there is a generally good path built by the Town Council and this demonstrates the potential of this railway corridor.



MAP 4 Bradford Road A6038



The railway path runs through a dramatic steep-sided 6m cutting. There are good opportunities for artwork and seating to overlook the neighbouring golf course just prior to this section. The tarmac path would be set on a low causeway to maintain drainage

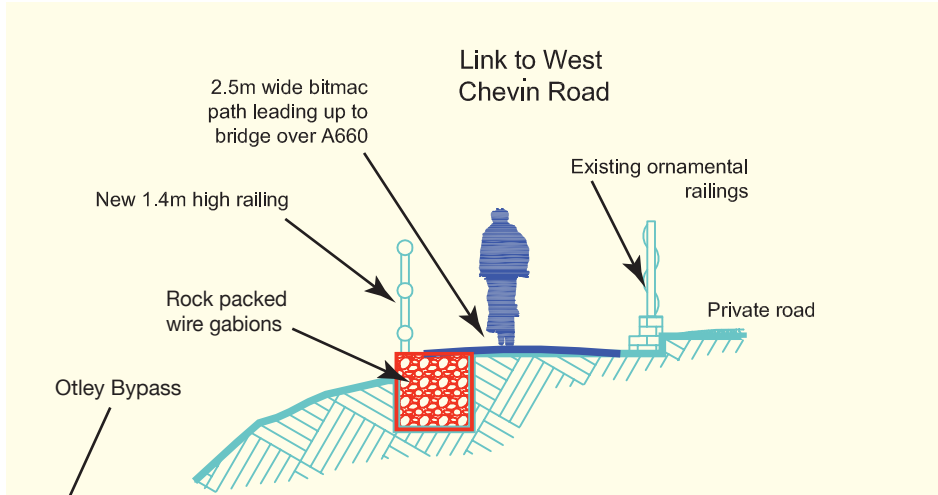
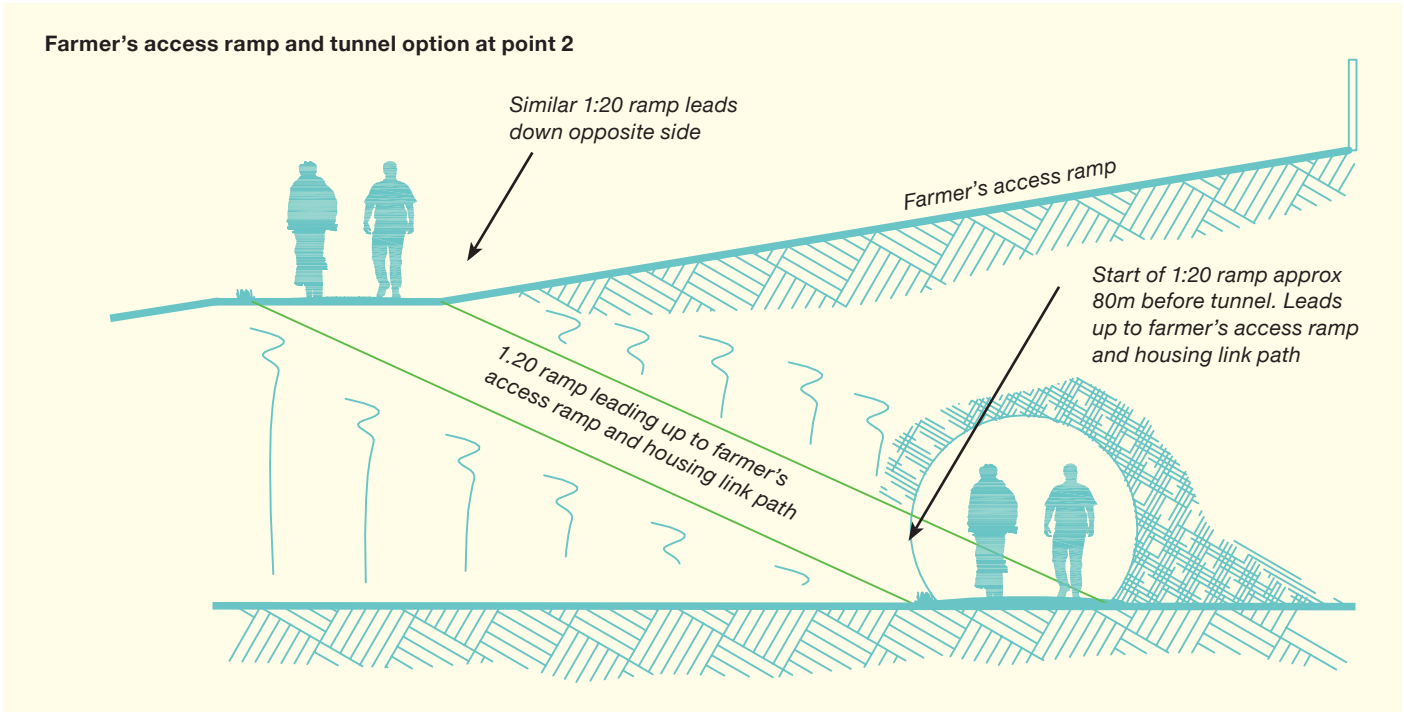


View looking through existing footpath bridge towards Otley showing the generally good path already in place

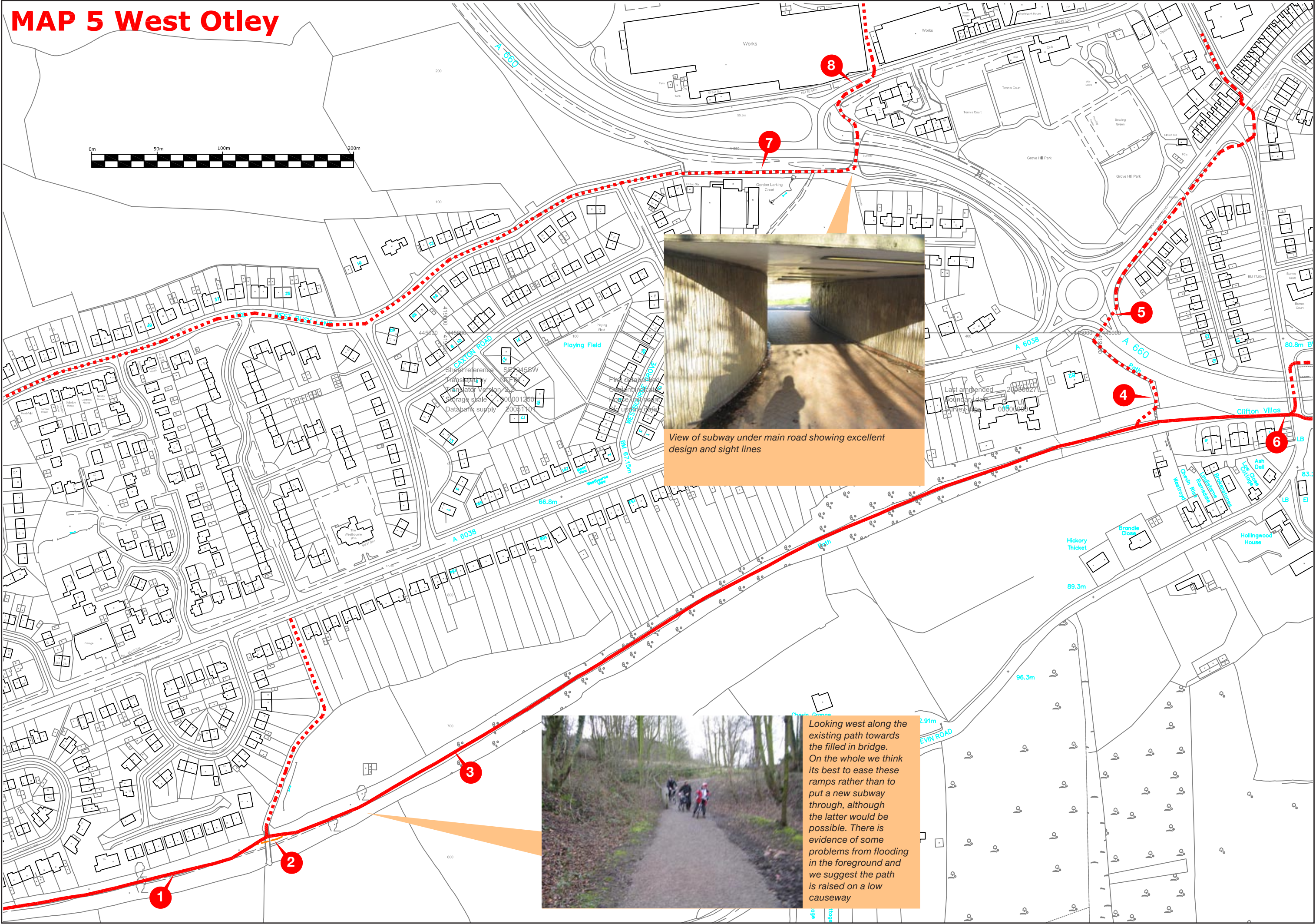
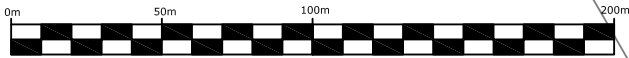
Map 5. West Otley

1. The path continues with a good stone base, which we suggest can be improved with a sealed surface as discussed in the technical section.
2. This former bridge is in-filled and we suggest the construction of a long ramp either side as shown in the adjacent sketch to give both good continuity and a good connection to the existing, well-used green lane leading down to the main road. There is also the potential to construct an Armco type tunnel through the in-fill of the former bridge. This, together with the linking ramps to the green lane, would be the best option for directness as well as connectivity.
3. To the east of the bridge there is evidence of problems with drainage in the cutting and we suggest that the reconstructed path is built on a low causeway to avoid this in the future.
4. The existing end of the path comprises a short flight of steps which precludes its use by cyclists and wheelchairs, however there is ample room here to construct a good and evenly graded ramp down to the splitter island on the main A660 road.
5. This splitter island needs to be slightly modified to provide for a two way cycle route from the A659 Bradford Road to the NE of the roundabout. This will necessitate using a short length of the south side footway probably as far as Inglewood Drive in order to create a safe crossing of Bradford Road to reach the town centre.
6. The critical link required is a new path below Clifton Villas linking on to West Chevin Road as shown in the sketch below. This is crucial for the ongoing route to the town centre via Birdcage Walk.

7. This underpass connecting West Busk Lane with the Ilkley Road is a most valuable link for people from the west of the town. It could be signed as a shared use route now.
8. Flush kerbs needed here to make connection to road.



MAP 5 West Otley



View of subway under main road showing excellent design and sight lines



Looking west along the existing path towards the filled in bridge. On the whole we think its best to ease these ramps rather than to put a new subway through, although the latter would be possible. There is evidence of some problems from flooding in the foreground and we suggest the path is raised on a low causeway

Map 6. Along North bank of River Wharfe

The River Wharfe has played an important role in Otley's history as an industrial paper making town. Although the local economy has shifted its focus away from traditional riverfront activities, the river still offers a valuable resource to the local community. Indeed, in 2004, Otley Town Partnership commissioned a study to create a vision for Otley's Riverside. Its aspirations fit very neatly with those of this study and its principal features have been adopted here.

Otley Bridge

In order to fully appreciate the river frontage and ensure any route is valuable it must be well connected with the town centre. Otley Bridge is obviously the focal point for this connection but is highly constrained by the available widths that do not currently allow for convenient or safe cycling within the carriageway or footway. Pedestrians are adequately catered for but any future alterations to the bridge layout must consider the needs of cyclists if Otley's greenway network aspirations are to be fully realised.

In 2008, proposals were put forward by Leeds City Council to widen Otley Bridge, remove the adjacent footbridge and carry out essential strengthening, waterproofing and masonry work and a public consultation exercise was conducted. The need to increase the safe width for pedestrians is welcomed but the need to increase the bridge width for vehicular traffic seems in direct conflict with the ability of Otley Town Centre to cope with more and heavier traffic. It is our view that the carriage way should not be widened but safer facilities for walkers and cyclists should be provided within a widened footway. The idea of providing an entirely separate bridge adjacent to the historic structure seems to have been dismissed on cost grounds. However, this would ultimately provide the safest crossing for walkers and cyclists whilst allowing the historic bridge to be returned to its former appearance. Sustrans has a great deal of experience in providing new, cost effective bridge crossings and we feel that this idea deserves further investigation as soon as possible.



View to north of Otley Bridge footway

From Ilkley Road:

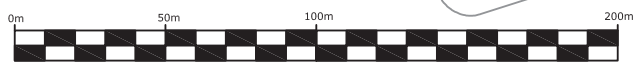
1. An existing bridleway links Ilkley Road with the riverbank.
2. A new footbridge over the River Wharfe is needed to link the new developments around Otley Mills to the proposed north bank path. This development would enable all the residents on the north side of the river in Otley to enjoy safe, direct and easy access to the greenway in the westerly direction and is therefore of significant importance. A new bridge would also contribute to the regeneration of the riverfront as well as provide a gateway feature for the town.
3. The preferred alignment of the north bank path would require a new sealed surfaced path to be constructed adjacent to the river bank. Subtle landscape screening may be required to provide a barrier to the agricultural land.
4. A private access track belonging to the sailing club exists close to the northern bank of the river. If an interim alternative is required to the new riverside path, it may be possible to negotiate a permissive right of way along this track to provide the link to Billam's Hill. Patching and resurfacing work would be required to make this suitable for cyclists and wheelchair users. The fantastic opportunity to provide walking access, viewing points and interpretation boards for the nature reserve should not be missed as this is an amazing asset so close to the town centre and is currently restricted to the public.
5. A toucan or cycling zebra crossing of Billam's Hill would provide a safe, continuous route to Wharfemeadows Park and on to Prince Henry's Grammar School.

6. On the southern side of the river, an existing surfaced path through Manor Garth Park and link to St Joseph's Primary School provides a convenient and attractive link to Westgate. This path would become even more valuable if it was linked via a toucan crossing of Bridge Street to Mill Lane and then on to the Garnetts Mill development site. This site is likely to be retained as an employment centre and so it is vital that it is linked into this greenway scheme's aspirations.



Good surfaced path in Manor Garth Park provides a very valuable link to Westgate

MAP 6 Along North Bank of River Wharfe



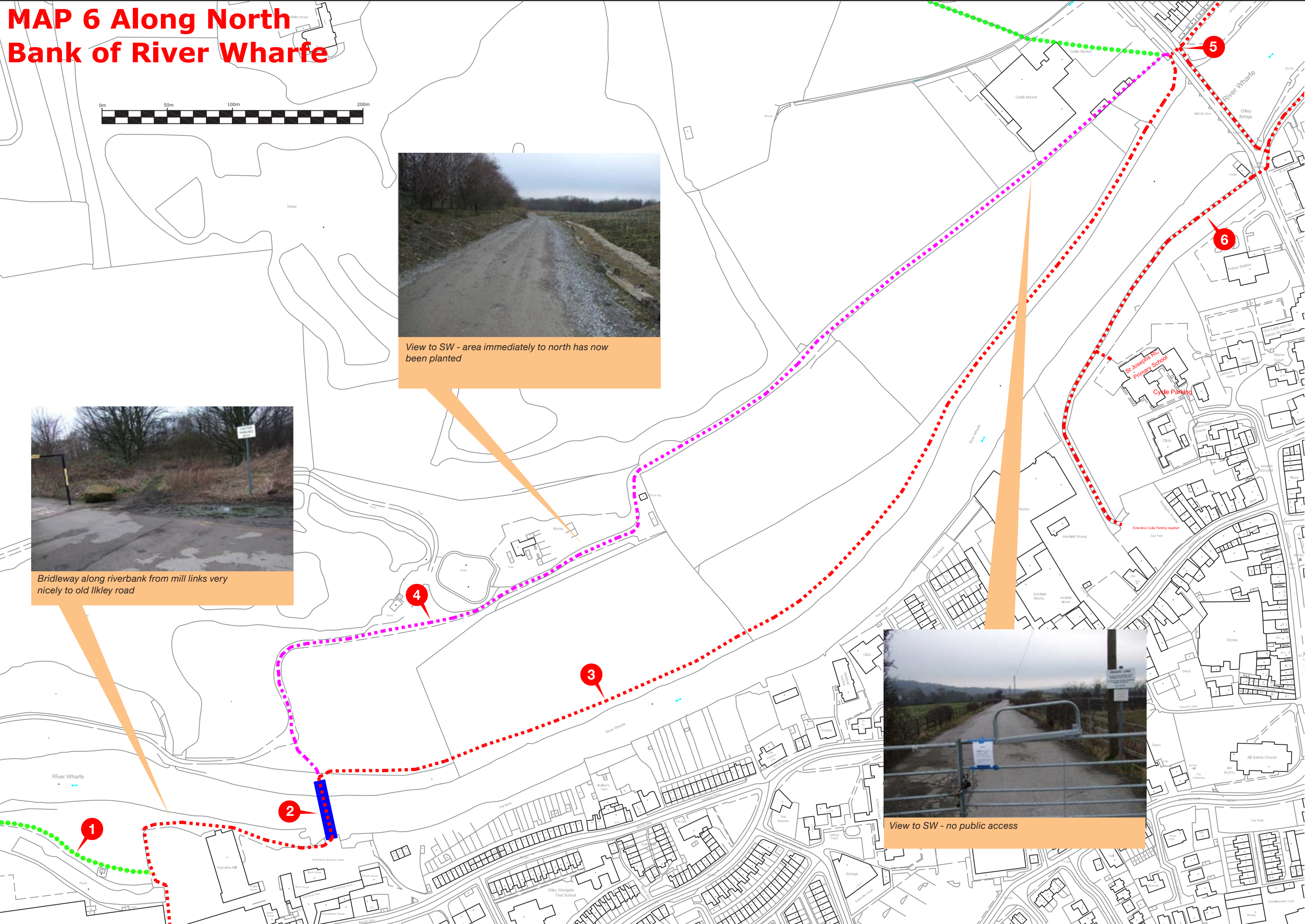
View to SW - area immediately to north has now been planted



Bridleway along riverbank from mill links very nicely to old Ilkley road



View to SW - no public access



Map 7. Otley

The route now meets the challenge of the 60mph A660 Otley bypass. Two options are considered in detail here. A third option of creating a roadside verge route alongside the south side of A660 was considered but dismissed as inappropriate due to its proximity to fast moving traffic and its complete lack of convenient connectivity options with Otley town centre.

- 1. Birdcage Walk is an attractive road avoiding the bypass. It is said to be a bit of a rat run and measures to calm traffic by making it a 20mph road would be valuable.
- 2. The existing footbridge is well suited to shared use having a level ramp on the south side and a zigzag ramp on the north side leading to the old station, Station road and the town centre.



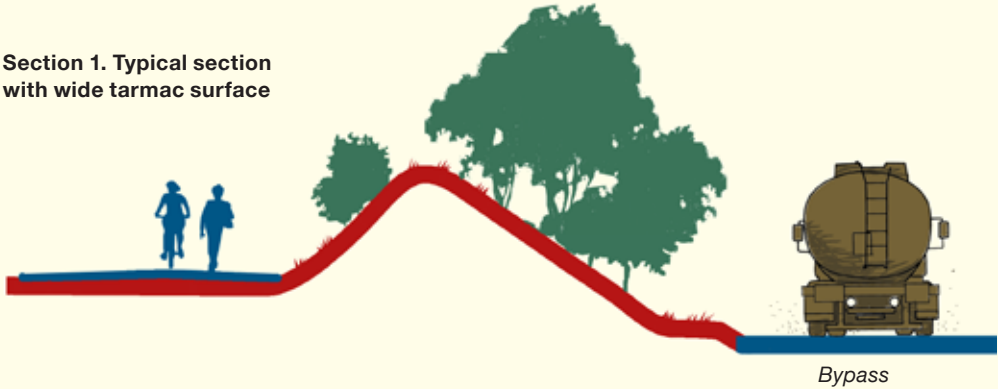
View looking up the zigzag ramp leading to the bridge over the A660. On the south side, the bridge runs into the lane up a short, well-graded ramp. The parapet heights are slightly low for cycle use at approx 1.2m and so we suggest that for the span over the road itself, it would be sensible to raise their height to 1.4m by clamping an extension onto the existing rails.

- 3. As so much of the station area remains intact we did investigate a route parallel to the road on its north side. This would be quite difficult due to the private drive here coming so close to the road. If use of the drive could be negotiated to a point somewhere near the garages then it would be quite straight forward to construct a route along the north side of the noise bank linking West Chevin Road with Station Road. The adjacent cross section sketches show how this could be achieved. Such a route would have the advantage of being almost completely traffic free and make use of extensive areas of existing public space that is currently relatively un-useful. Landscaping, interpretation and art installations could transform the former station site into a destination in its own right.

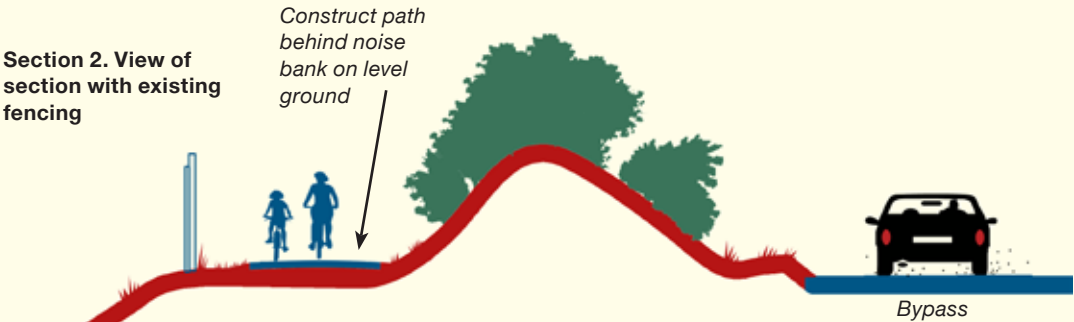
- 4. If this north side route was pursued by the Council then it would be desirable to make a link into Whitely Croft Road as this would give access to the railway path for a large number of residents in the south part of town.
- 5. The route onwards could continue along Birdcage walk to Fairfax Street although it would require some quite subtle thought on how best to cross Fairfax Street to East Chevin Road when travelling westwards on account of limited sight lines or from the centre of town via Bondgate as shown on these plans. We imagine that this will be the more normal route as people will be travelling from Pool in Wharfedale to Otley Town Centre for shopping etc. Here the key link is the existing good path from Well Croft to Cambridge Terrace and thence via Carlton Street for another important link across to Croft Street and Lisker Drive.

Four Views North of the Bypass

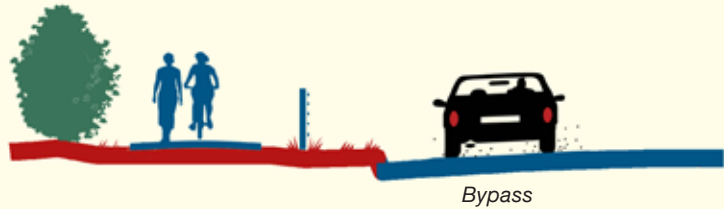
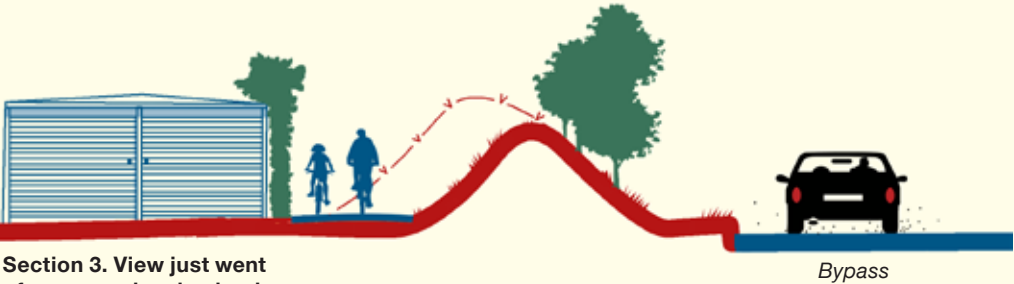
Section 1. Typical section with wide tarmac surface



Section 2. View of section with existing fencing



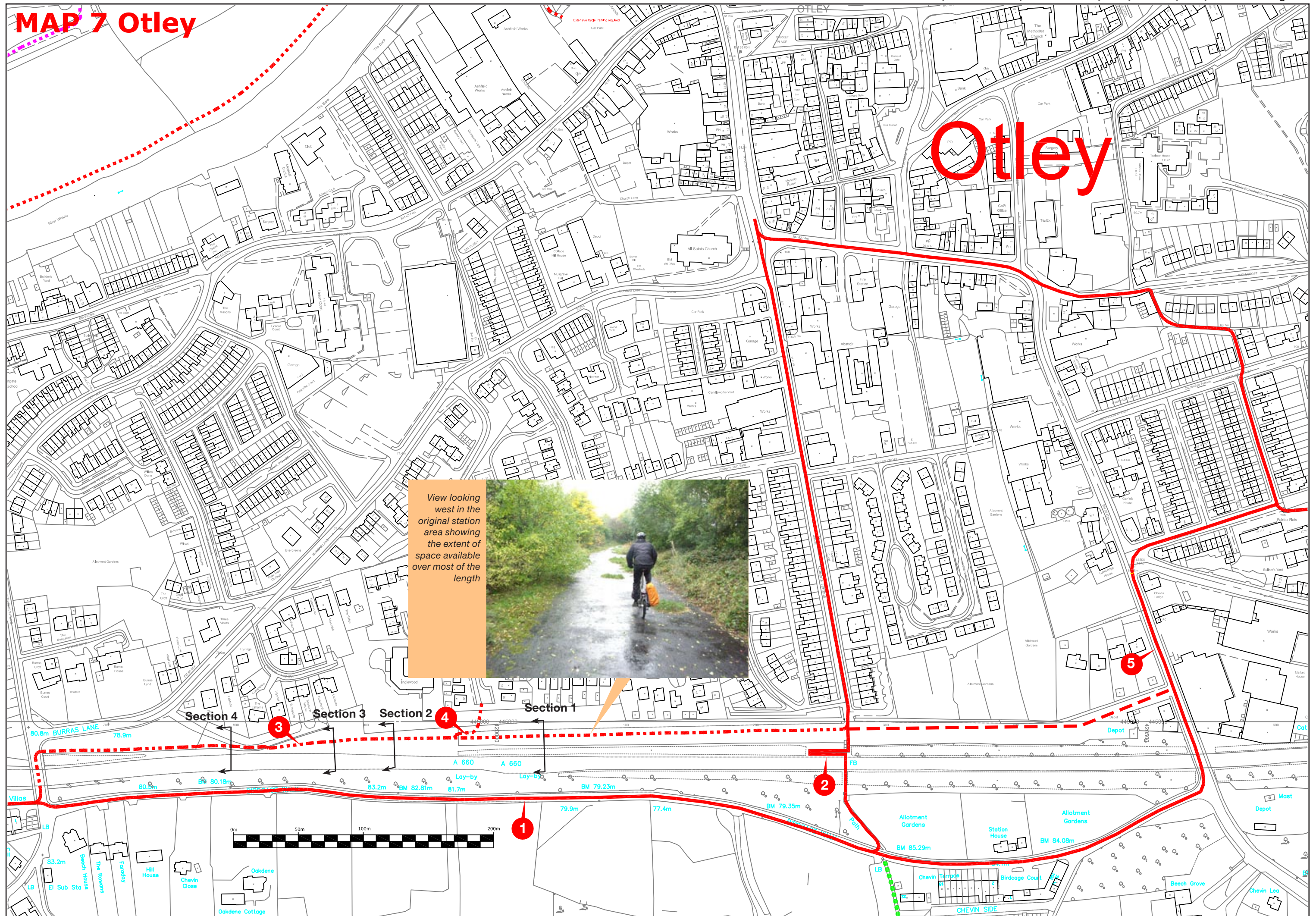
Section 3. View just west of garages showing bank sliced away. To allow path through



Section 4. View showing the access road to the access road to the garages being used by the public, just west of a possible new gate



View looking west in the original station area showing the extent of space available over most of the length



Map 8. White Bridge

- 1. Prince Henry's Grammar School entrance deserves a wide toucan crossing link into the park.
- 2. A new section of path cutting across the open space would give a well graded link from the school entrance down to the existing riverside path.
- 3. This attractive route leads along the banks of the Wharfe and would be a memorable part of any journey either to school or for recreation.



- 4. White Bridge is a most attractive crossing of the river and already has well built ramps either side quite suitable for shared use. Note this bridge carries the sewerage over the river and the ramp arrangements are such to avoid these pipes.



The White Bridge which has excellent ramps either end



View from White Bridge

- 5. The paths here are relatively complex in that they go around the boundaries of former sewerage works, pass under the pipe with limited headroom or run beside it. We suggest that it should be possible to agree rerouting the main paths to cut the corner as shown and cross over the sewer at the northern tip of the cemetery.



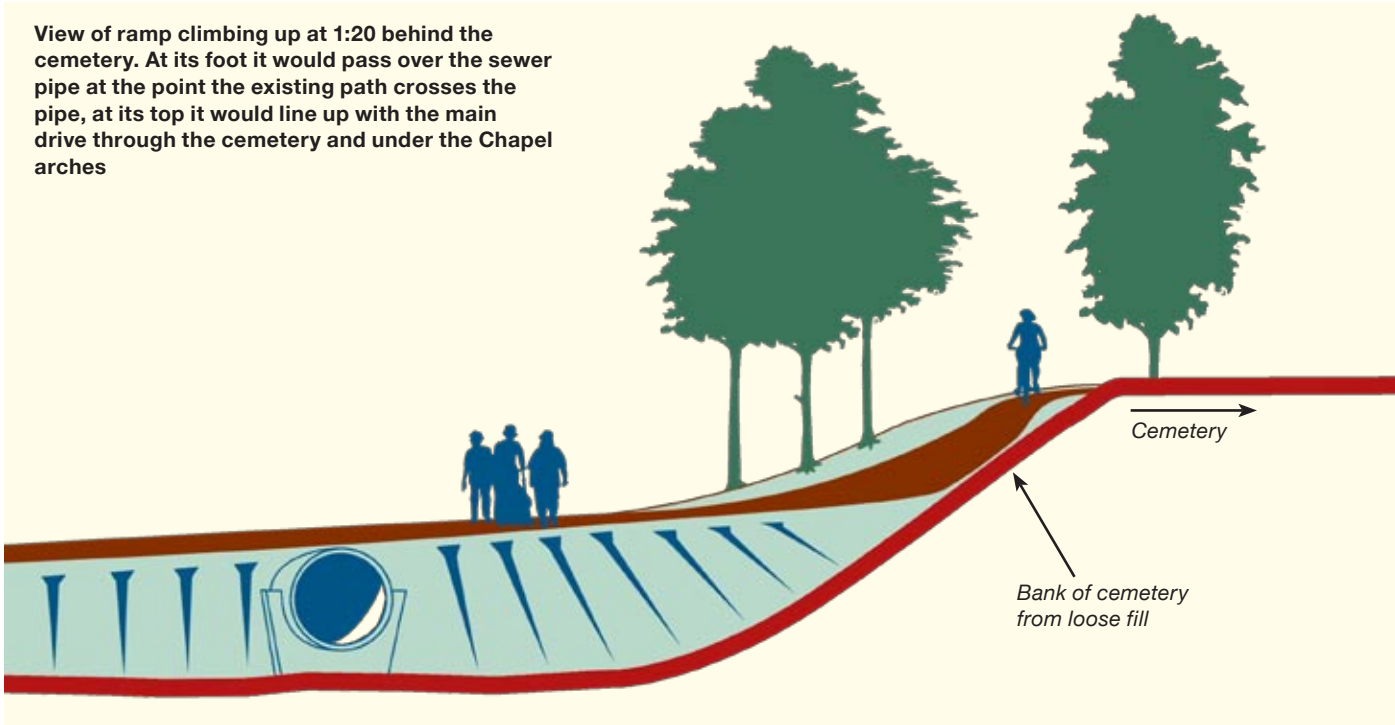
The existing path has to cope with running beside and under this main sewer. The proposed new arrangement would cross the pipe at the top of the ramp shown in the distance

- 6. From here a new ramp should be constructed by adding material to the steep bank leading up to the cemetery in order that the path can connect to the main drive through the cemetery under the chapel arch and through the main road at Cross Green.



View through the cemetery chapel showing the rather magnificent vista leading down to the Pool Road with the Green beyond

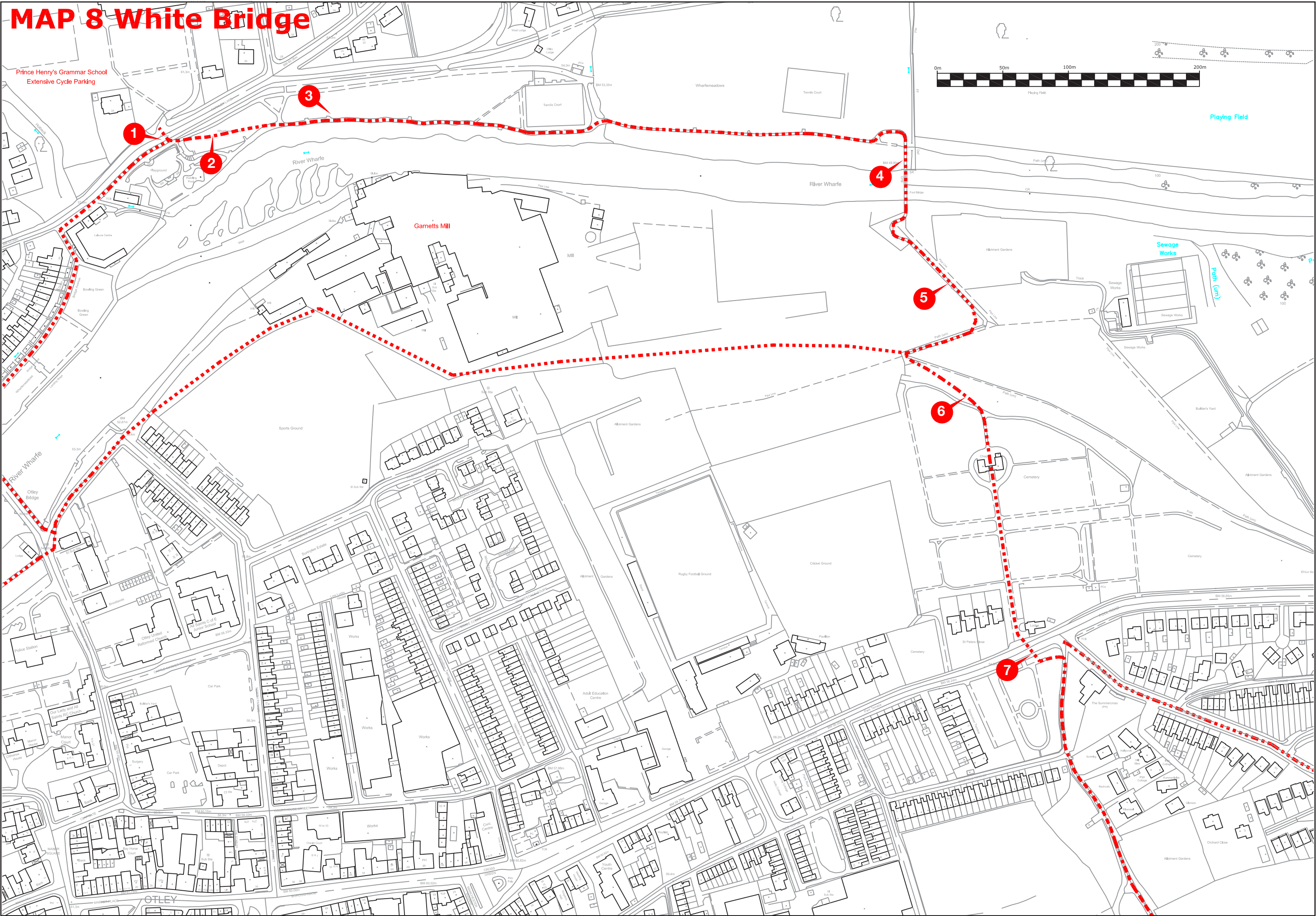
- 7. A toucan crossing is needed over this road to reach Cross Green and then a short length of new path to link to Ings Lane and East Busk Lane for routes to east Otley and Pool in Wharfedale respectively.



View of ramp climbing up at 1:20 behind the cemetery. At its foot it would pass over the sewer pipe at the point the existing path crosses the pipe, at its top it would line up with the main drive through the cemetery and under the Chapel arches

Cemetery

Bank of cemetery from loose fill



Map 9. East Otley

1. Lisker Drive is almost level and if planted with avenue trees would make an attractive approach to Otley and provides an important link to All Saints Primary School.
2. There are existing field paths connecting Lisker Drive to the railway and the best arrangement of these would need to come out of discussion with the landowner, taking account of field boundaries and the need to create as well graded a ramp as possible. Either way the path should be aiming for that point at the east end of the former cutting (now filled by the Leeds Road roundabout) where the ground is level with the fields. Alternatively hug the boundary to the west and cut a new evenly graded ramp up to the railway.
3. The next section of railway is most attractive, heavily wooded, and a feature in its own right.
4. This link path from Ings Lane to Cambridge Drive is a key connection across to White Bridge and the Grammar School. Ideally it would be constructed as a continuation of Ings Lane and tree lined through to Cambridge Drive. This precious link into the residential roads would have to be negotiated and would be best by one of the garage forecourts.



Well worn path leading up towards the residential area

5. East Busk Lane is an attractive rural road which provides the direct route from Pool in Wharfedale to White Bridge and the Grammar School.
6. Masonry arch bridge in reasonable condition. New steel parapet rails might be desirable rather than the present ones.

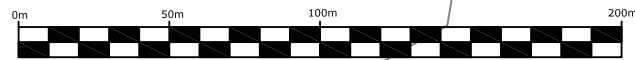
MAP 9 East Otley



The track continues into an open field



View looking along East Busk Lane towards Otley



Looking west along the existing path towards the filled in bridge. On the whole we think its best to ease these ramps rather than to put a new subway through, although the latter would be possible. There is evidence of some problems from flooding in the foreground and we suggest the path is raised on a low causeway

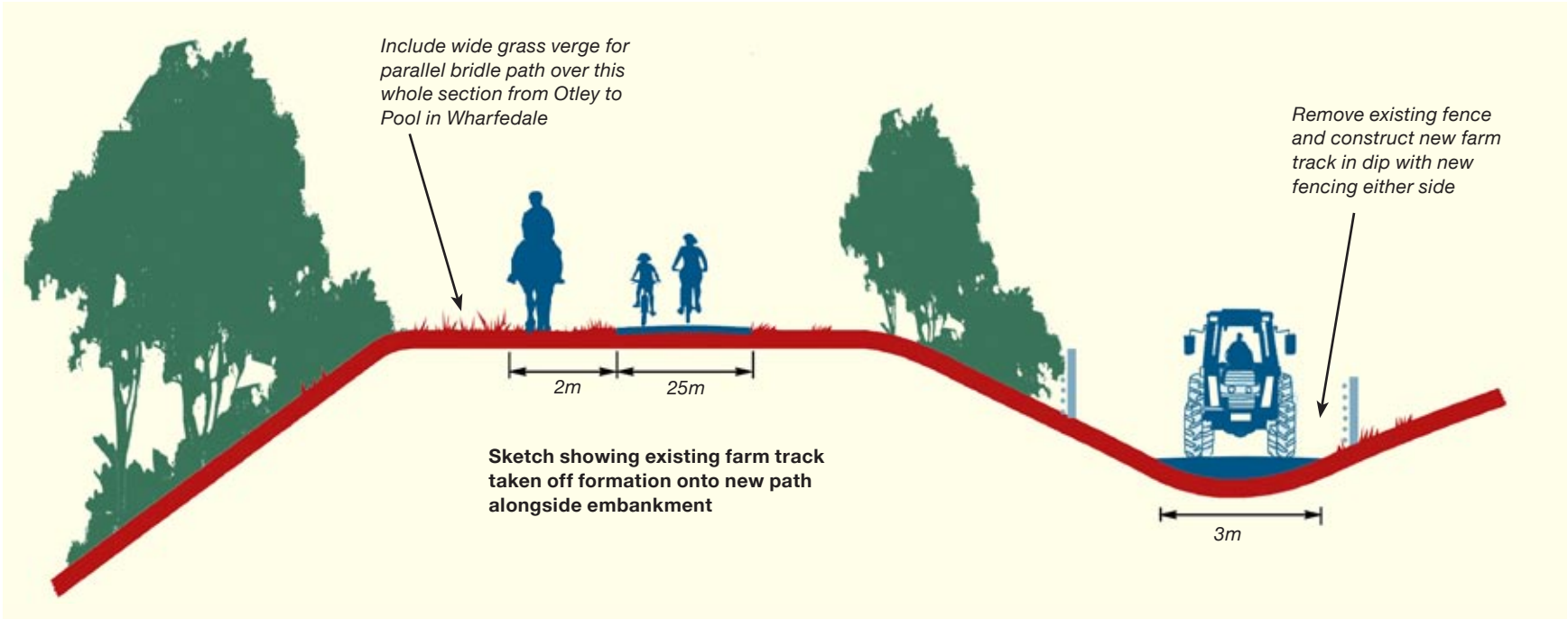
Map 10. East Busk Lane Link to Railway Path

1. Route continues along railway path to the link and this masonry arch is in good condition.
2. At present a short length of the track is used for farm access to buildings further east. Ideally this track would be reconstructed parallel to the path.
3. The connection to East Busk Lane is most important and this needs to be carefully graded.



View under bridge. The public footpath runs through there, as does a twin slabbed water culvert. It would be desirable to tidy all this up and probably seal the surface to ensure no further damage from flood water

4. The path itself reconstructed to be more than passable in all weathers. The footpath goes on under the bridge.
5. If possible a link needs to be negotiated southwards perhaps via the Stubbings Farm access road in order to provide a link to the Leeds Road, across it and up to the forest park – at least for walkers.
6. Over this section great care should be taken to ensure the path is dry throughout by constructing on a causeway through cuttings. At the same time views over the Wharf Valley need to be maintained and vegetation managed accordingly.
7. There are a number of interesting bridges under this line, each of which provides useful viewpoints. Each time their parapet walls need to be repaired and extension balustrades provided either end.



8. This missing span crosses an unused accommodation bridge. If this is the case then the gap could be filled with earthworks and the masonry recovered for use elsewhere.

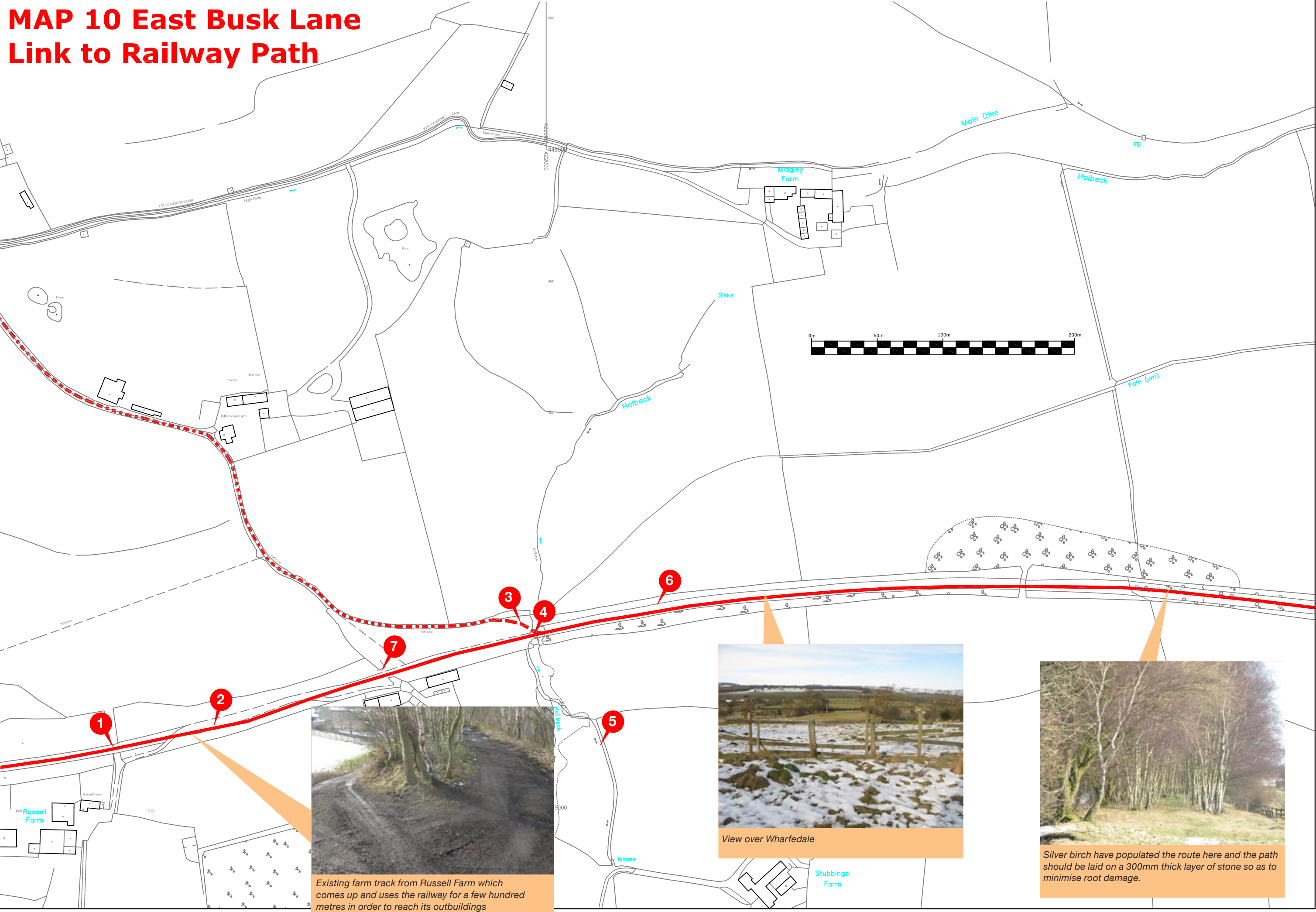


View of missing bridge which we suggest is in-filled, having first recovered the masonry to use on bridge repairs elsewhere along the line



The preferred arrangement for incorporating a bridleway is to run the path for horses alongside the tarmac path, separated by a grass verge

MAP 10 East Busk Lane Link to Railway Path



Page 24 opposite

Map 11. Caley Farm

1.

Passage along this section of the railway will depend upon the agreement of the landowners and the arrangement of the path will need to take account of their preferences.
3.

The remains of concrete installations here appear to have been abandoned and will be buried from sight by the proposed causeway.



2.

The cutting under the two remaining bridges is greatly flooded and if the path were to go this way it would have to be constructed on a substantial causeway as shown in the sketch. This effort is well worthwhile because it ensures that the public are kept well separated from the day to day traffic on the farm itself. At the same time a private link is needed to the farm to allow the railway path to be used by its residents and in particular by equestrians.

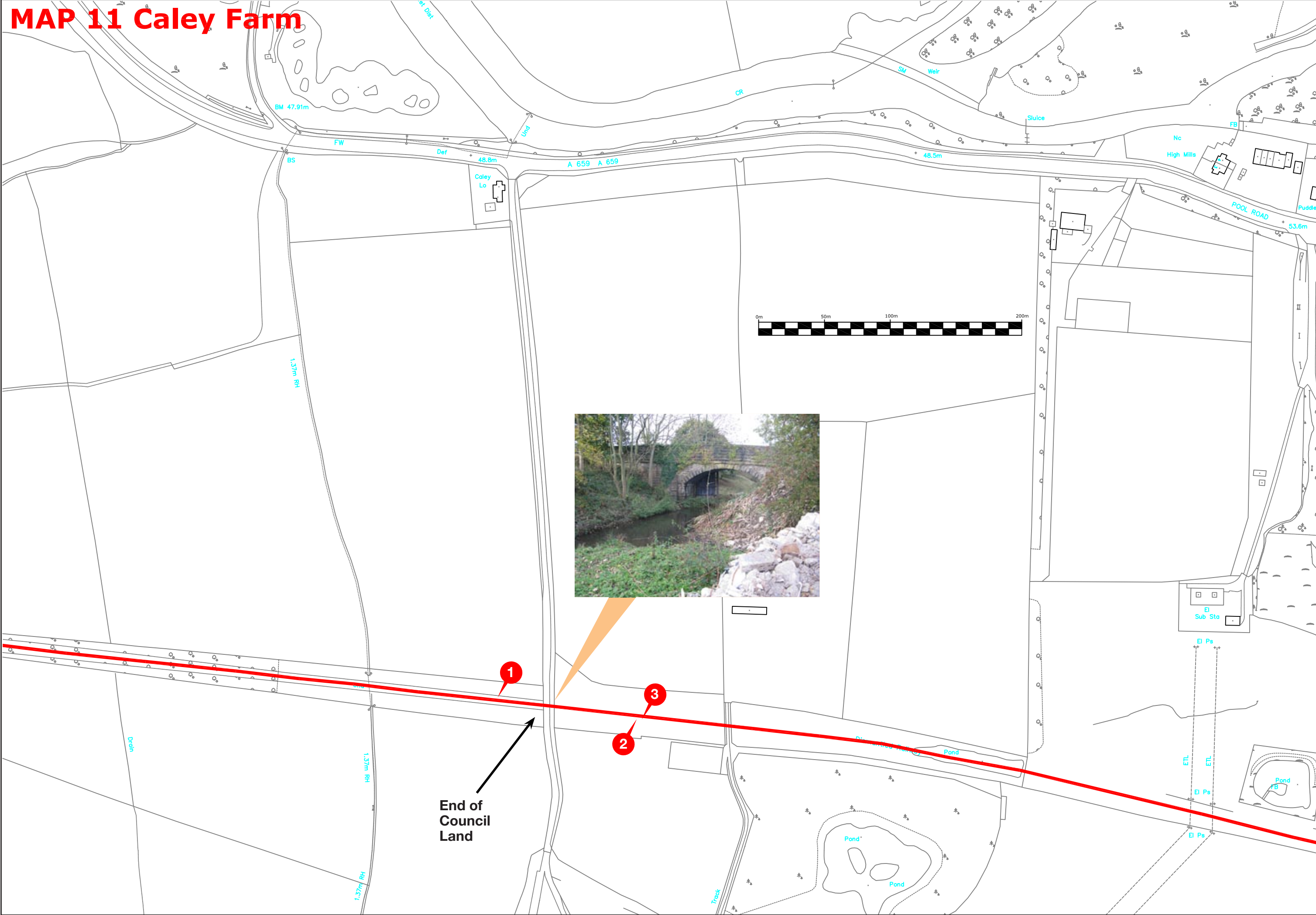


Looking East from Caley Farm Road showing flooded cutting



This example from Scotland shows how the path built on a causeway can allow the flooded cutting to become an attractive feature

MAP 11 Caley Farm



Page 26 opposite

Map 12. Pool in Wharfedale

1. The railway continues with good views over the surrounding countryside.



Looking west along former railway embankment

2. Because the railway route does not connect conveniently with Pool in Wharfedale a new link will be required to reach the centre of the village. The best link would appear to be along this field edge to reach Church Lane with a branch into the garage forecourt to service Church Close.



Looking back from field edge to railway embankment

3. The ground here is wet and the path should be built on a low causeway, ideally planted with trees on the side away from the existing hedge in order to create a green lane effect.



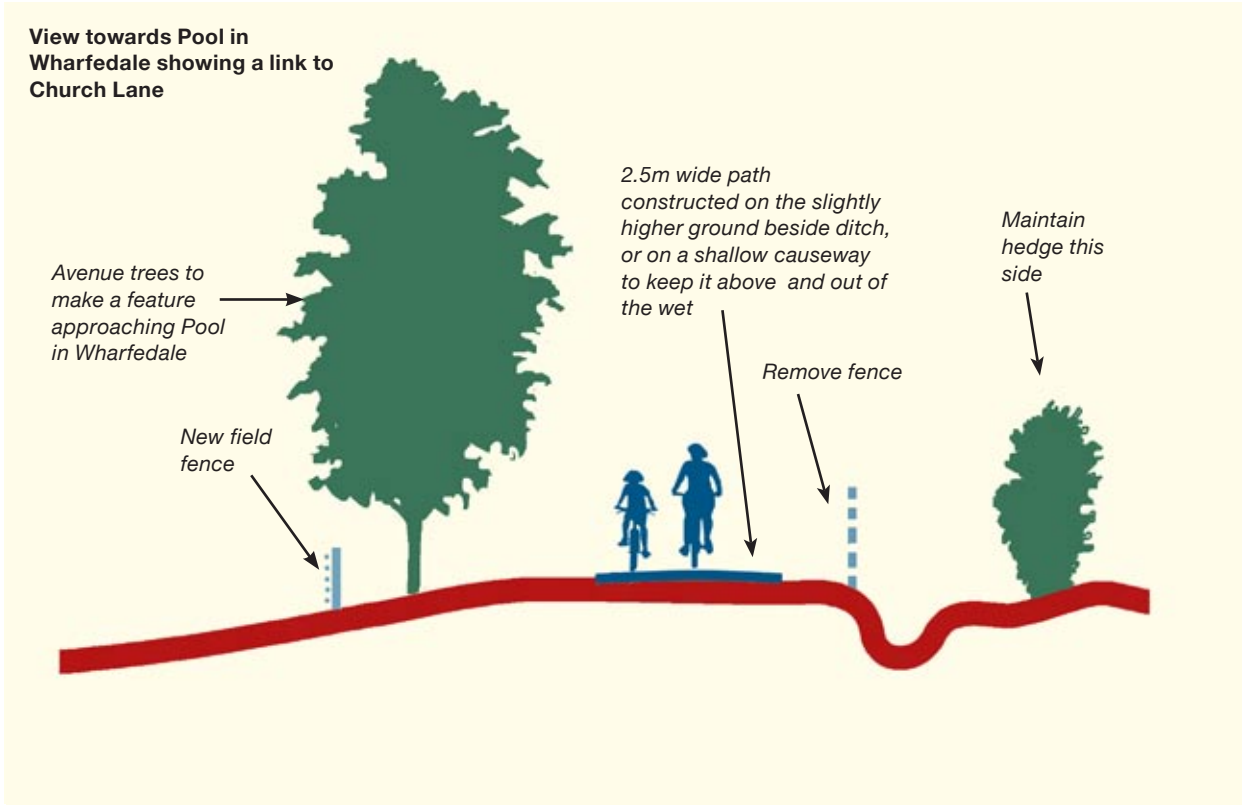
Field edge after leaving railway looking towards Pool in Wharfedale

4. Agricultural crossings need to take priority here and any gating arrangements need to meet the landowner's agreement.
5. The path needs to be taken along the field edge to Church Lane and again thought given to the type of boundary which in the sketch here is shown as a fence.

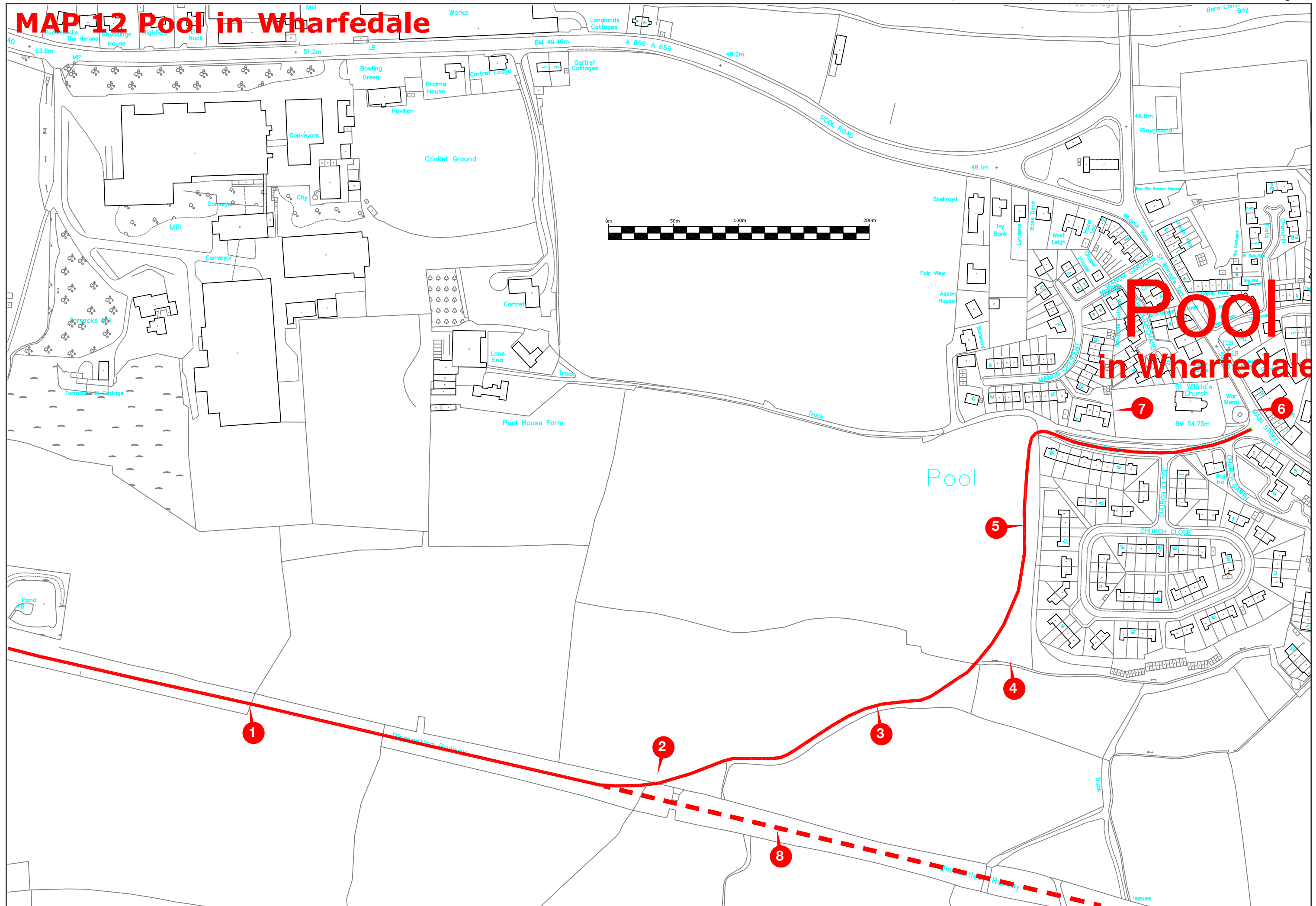


Route runs along field edge

6. The existing crossing of the Main Street is very useful and should be utilised to link to the eastern side of the village at the Bungalow.
7. If possible a link to Manor Crescent should be made via the edge of St. Wilfred's Church Yard in order to allow local residents here to reach the path without having to follow the main road.
8. Route continues to Pool Bank and the east end of Pool in Wharfedale. Please see Appendix 1: A note on links to Harewood House for further route development scope outside of the main detail of this report.



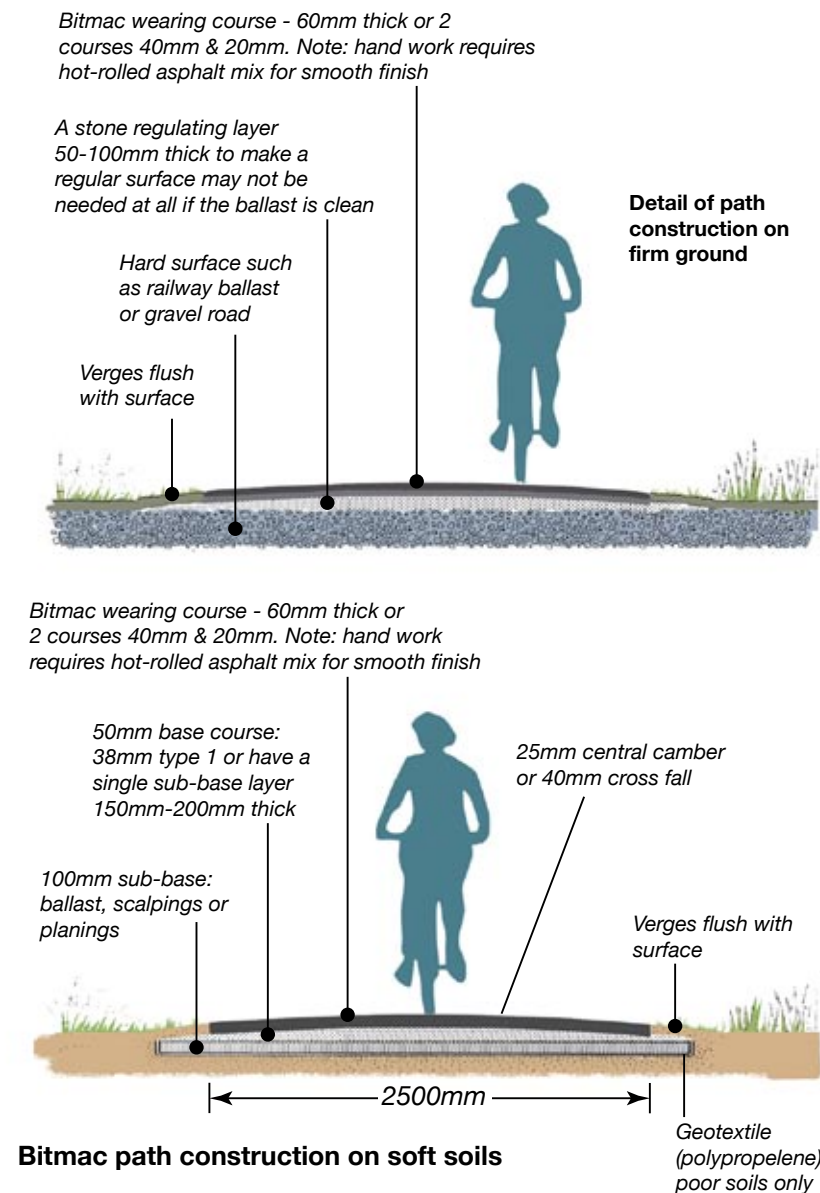
Pool in Wharfedale



5. Technical Notes

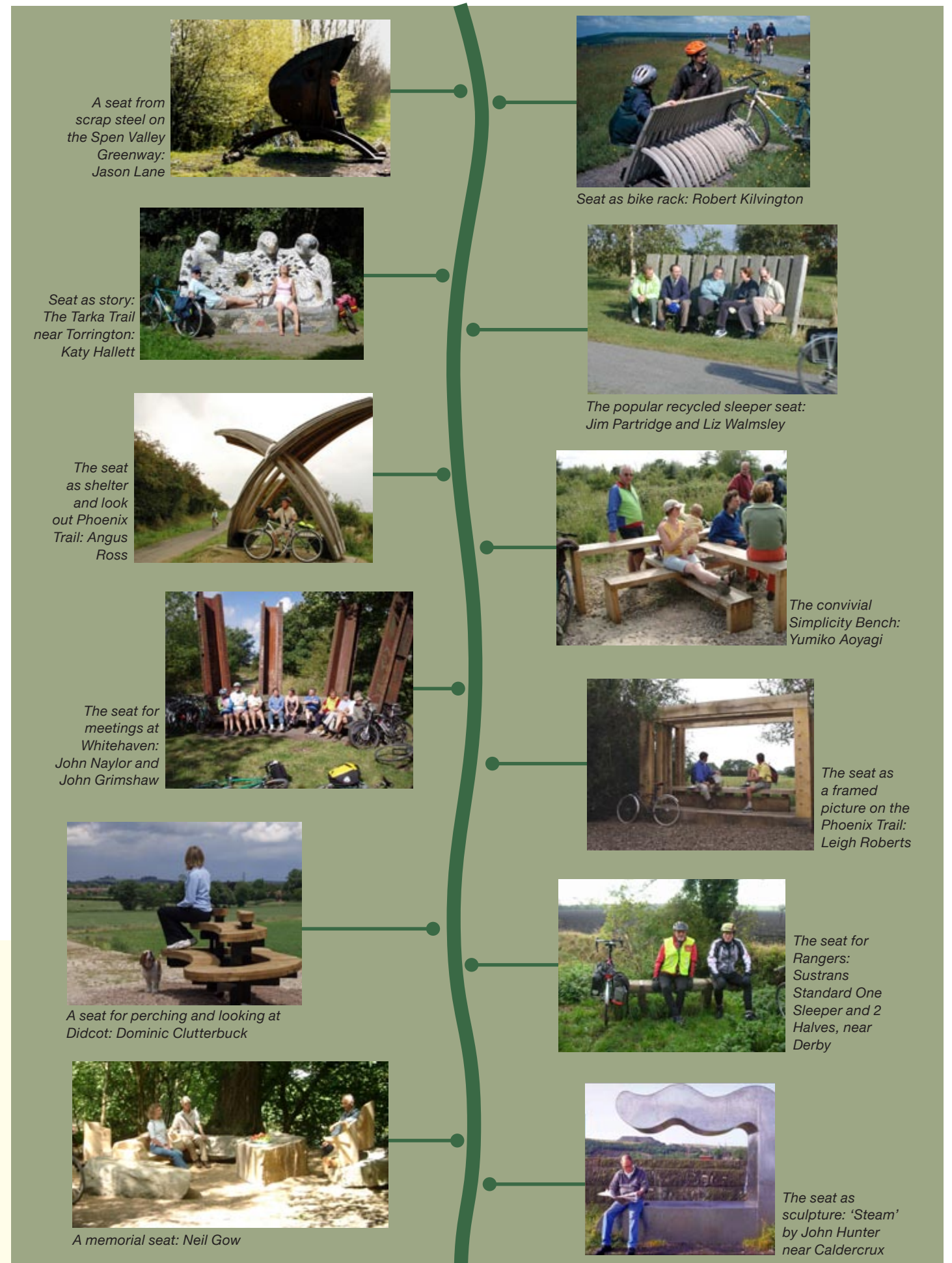
Path Surface

Path surfacing is probably the single most critical element determining the popularity of Greenways! A surface which is smooth, firm and dry throughout the year and throughout its lifetime will generate far higher levels of use than will any sort of informal surface which is prone to damage from water, erosion and even horses. On this project we recommend laying a dense bitumen macadam (DBM) surface. This should always be machine laid and generally a single 60mm layer is the most appropriate solution. (If a second layer is used then the weight of the construction vehicles laying this second layer may well damage the first layer, especially on soft ground).



Seats

Part and parcel of walking and cycling is stopping and resting. Seats should always be carefully positioned so they have a particular view, or are under a particular tree, so that each and every seat is a place where a person might actually want to stop and, perhaps even more importantly, a destination a person might want to reach and a locality where they can meet friends. At entrances, or in areas likely to be used by a fair number of elderly or disabled people, seats should be close together, no more than 200- 300m apart, because even these distances might present quite a challenge for some. Seats should be positioned not only so they have an attractive view out over the vista either side of the path but also along the path so that their very presence and indeed occupancy leads to informal surveillance of the Greenway itself.





Anti motorcycle access barriers can be absurdly complex and are a complete obstacle to legitimate users



Open access without barriers at the Eden Project's access road shows the ideal entrance to a Greenway - both unrestricted and with a priority crossing of the road



Bollards should be used to prevent vehicular access – York University. Note the removable unit for vehicle access



At Workington large local rocks have been used to define the route and create chicanes

Access

Although our ambition is that all Greenways should be freely open for walkers and cyclists without barriers, there are many instances where stiles, gates or access controls of some kind or other are required. These are needed for the control of livestock where the Greenway passes in and out of grazed areas, for the provision of maintenance vehicles which require larger entrances than walkers and cyclists themselves, or for arrangements of various kinds to deter motorcycles.

Bollards with a spacing of 1.20 – 1.50m can be used to prevent motorised vehicles accessing Greenways or other physically separated paths while still allowing comfortable access by cyclists, pedestrians and wheelchairs. Locked removable bollards can be used to allow access by maintenance vehicles. If the Greenway is also used by equestrians on the same alignment, the bollards need to be 1.80m apart.

Similar effects can be realised by the use of rocks with similar spacing or by creating chicanes with rocks.

Other types of access barriers should preferably be avoided. While it is recognised that use of motorcycles presents a problem on some sections of the National Cycle Network, the use of other types of access barriers restricts and deters legitimate users such as cyclists and wheelchair users as much as they do motorcycles.

Views

Views can be divided into those along the Greenway in the direction of travel and those to one side or the other of the route itself. The pictures in this section are all taken from a single Greenway – the railway path from Drumgelloch on the outskirts of Glasgow to Bathgate on the way to Edinburgh. As this was a relatively straight railway traversing a somewhat bleak landscape considerable effort was devoted to making it as interesting and memorable as possible.

The path was moved from side to side to break up long forward views. The route was moved to the boundary to give clear views over interesting countryside, or raised onto adjacent ground for the same reason. The photographs and text here describe these devices which have wide applicability.

The long straight alignment towards Blackridge made more memorable by shifting the path from side to side



Deviating the path to make a central feature of a milepost - 'Bedrock Bike' by Dave Holladay

A gentle ramp adds to path interest and is aligned with a view of the Loch. Notice the radiused junction



At Armadale the Greenway moves off the railway to take advantage of the remains of a Green Lane and its trees

Notice the profusion of wildflowers flourishing in the verges where no topsoil is used



The dramatic profile of Lumir Soukup's 'Bathgate Face' breaks up the shape of a large mound of waste earth



Blocking off the long view with mounds from waste material



Aligning the path right to the side of the formation so as to maintain views over Hillend Loch. The bank on the left to shield noise from adjacent road



The path diverts off a high embankment to gain shelter from the wind and to hide from views of an industrial site. This allows the Blue Legs by Doug Cocker to be appreciated the better



On the way to Balerno the Greenway follows an old county road, now traffic-free



A simple bench near Drumgelloch provides a goal for the walk, a place to talk and to observe the passers by

Fencing

Stock fences are usually post and wire. It is best to install 7-wire mesh with 2 lines of barbed wire above. The mesh ensures that dogs don't get through to worry stock. The barbed wire should always be on the stock side of the posts with one line of smooth on the public side to reduce the likelihood of injury if a member of the public accidentally crashed into the wire.

The fence should always be as far from the path as possible so it is not casually tampered with. A minimum of 1m is highly desirable.



A post and wire stock-proof fence on a rural path at Chedzoy in Somerset. Note that fence is set 1m from path



Another view of the field fence at Chedzoy



On the Gloucester and Higham path a top rail was added to assist people with disabilities near to the village

Sculpture and Site Specific Works

Sustrans has had a long tradition of commissioning artists and sculptors to work on its projects. From the beginning the objective has been to:

- 1. Articulate the whole length of the Greenway and to give rhythm and points of focus on otherwise relatively featureless routes such as derelict railways.
- 2. Mark out the mileposts.
- 3. Provide local historical and geographical interpretation to enhance users' knowledge of the location.
- 4. Create a memorable route one would want to visit again.
- 5. Make local destinations that local people could be proud of.
- 6. Create a way for the community to be involved in the making of their Greenway by artists working locally with schools and others to create and maintain their own pieces, promoting the feeling of ownership of the Greenway



Terris Novelis by Tony Cragg near the site of the Old Consett Steelworks



The 'Blue Pipe' by George Cutts in York marks the waterworks pipe running under the path and 'surfaces' at various points along the route



'Giaus Sentius' by Gordon Young, a drinking fountain on the Bristol & Bath path



'Wheel of Drams' by Andy Hazell Hengoed Viaduct



'Rotate' by Trudi Entwistle, an interactive work on the Spen Valley Greenway



'Sentinel' by Jim Paulsen on the West Country Way

6. Estimated costs of the Burley in Wharfedale to Menston, Otley and Pool in Wharfedale Greenway Project

These estimates are based upon the costs of similar railway path construction carried out by Sustrans Greenway Construction Team in recent months, at Thornton, near Bradford, the Bradley Viaduct route near Huddersfield and recently the Guisley and Yeadon Path. The estimates include all the normally expected works required along disused railways, and include for all the details of landscaping, fencing, seats and signs necessary for complete and open a popular path.

Sources of Funds

Funds for this type of project can come from a variety of sources. For a route of this importance, the local authority can be expected to contribute from Highway and Council Recreation and Leisure funds. Match funding can be sought from a variety of grant givers and the scale of that funding will be dependent on each funder’s criteria and how this route fits them.

The presence of this route can be expected to have a significant impact on the lifestyle, health, positive environmental impact, transport and leisure needs of the local population and as such, should score very highly against typical funders’ criteria. Other similar projects in the region have been match funded by:

Landfill tax credits - for projects in the vicinity of landfill sites.

Aggregates levy - for projects in the vicinity of or affected by quarrying - Pool in Wharfedale and Otley based projects should score very highly for this kind of funding.

Links to Schools funding - for infrastructural walking and cycling projects that will promote a positive change in a school(s) travel behaviour.

Yorkshire Forward - for projects that will have an economic benefit to the local community.

Natural England - a number of different grants available for projects that improve our natural environment or access to it.

Railway Heritage Trust - for schemes that will preserve the railway heritage - the former Otley station site may be of particular interest here.

Funders and grant availability does change over time and so applications for funding need to be put in at the appropriate time i.e. when all consultation processes have been completed and route construction is to go ahead. Sustrans has a very good track record in attracting funding for schemes such as this and all avenues would be fully explored if commissioned to do so.

1.	Hag Farm Road to railway under bridge – 230m Allow for laying a 2.5m wide path in good stone finished with machine laid bitmac, with good verges, a new field fence with new gates for farm access at Hag Farm Road, @ £100/m	£23,000
2.	Surfacing full width under the railway bridge	£2,000
3.	Making new ramps up form Heather Rise, excavating 1:20 slope in embankment and putting surplus material in landscaped mound overlooking wildlife area	£2,000
4.	Construction of new railway path to Bradford/ Leeds boundary, 1,400m @ £100/m including all fencing and landscaping	£140,000
5.	Holme Park ramped link with 1.5m wide bitmac path	£5,000
6.	Menston Old Lane Bridge – raise new earthworks approaches either side to give 6m clearance, make concrete bridge bearings each constructed on 3 needle piles, order and crane in 20m span, proprietary Warren Truss bridge, 2.5m wide and finish off approaches either side	£90,000
7.	Make 2 ramps onto Menston Old Lane, each suitable for works access during construction and for maintenance there after – each 100m long	£15,000
8.	Burley Road/ Bradford Road bridge – 40m span on raised earthworks and bearings as far as Menston Old Lane	£130,000
9.	Make works access off main road and allow for earthworks to ramp up to the higher level line	£10,000
10.	Link to Beech Close. Allow for footpath diversion and new field edge path with livestock fencing and new gates, 27m @ £90/m	£24,000
11.	Allow sum for zebra crossing of Burley Road	£5,000
12.	Lane either side of Westbourne Railway Bridge – rebuild with a sealed surface, including side links and crossing of Westbourne Drive, 380m @ £100/m	£38,000
13.	Link to stations, new path in wide verge beside the unadopted Westbounne Drive, 320m @ £90/m	£29,000
14.	New railway path from city boundary to start of existing path on railway behind Bradford Road houses, 850m @ £100/m	£85,000
15.	Link to West Busk Lane, including for earthworks down side of embankment, new path behind golf depot, new bridge over Gill Beck, and secure fencing against golf course, 260m @ £120/m. Footway and Highway Improvements to Westgate	£81,000
16.	New bridge over Bradford Road, including for new concrete plinth on west side abutment to give 6m clearance, and approach ramp. No works needed on east side other than bridge seating – 40m single span, 2.5m wide	£120,000
17.	Rebuild existing railway path to A660 roundabout, including for earthworks to ease ramp over missing bridge and new ramp down to roundabout. Path to be on causeway where necessary, 1,300m @ £100/m	£130,000
18.	New link below Clifton Villas to West Chevin Road. Allow sum to include for a limited amount of work with gabions, a new balustrade and cutting through end of bridge parapet	£20,000
19.	Railway path from Lisker Drive to Pool Bank, 3,600m including links from Lisker Drive, and Fairfax Street, filling in missing bridge, Caley Hall Farm causeway together with all fencing, gates and new farm access road all @ £100/m	£360,000
20.	Provision for minor works to link through Otley - sum	£15,000
21.	East Busk Lane, reconstruction and ramped link to railway path – 300m @ £100/m	£30,000
22.	Link to Church Lane in Pool in Wharfedale, ramping down off railway and running along field edge, including for ditches, culverts and farm access crossings together with livestock fence throughout, 500m @ £110/m	£55,000
23.	Link to Grammar School via White Bridge.	
	– Allow for zebra crossing of Pool Road and short link path in park	£10,000
	– New ramp from cemetery and link to bridge on causeway, 200m @ £150/m	£30,000
24.	New link in park to give well graded access to school together with zebra crossing of road - sum	£15,000
25.	The Cambridge Gardens link, 300m – sum	£30,000
26.	The Otley Mills Bridge, improvements to Otley Bridge and Developments around Garnetts Mill are each significantly important to the success of this scheme but further information and investigationis required to acurately cost these items	
	Total	£1,497,000
	Add 5% for seating, planting and signing	
	Add 5% for sculpture and features	
	Add 15% for insurance, land, engineering and management	£375,000
	Total net of VAT	£1,872,000

7. Programme

At this stage the programme can only be sketched out. The project is likely to be delivered in stages, with the most useful parts first. Although each section described in this report has real value in its own right, the section linking Otley town centre to Menston Station for

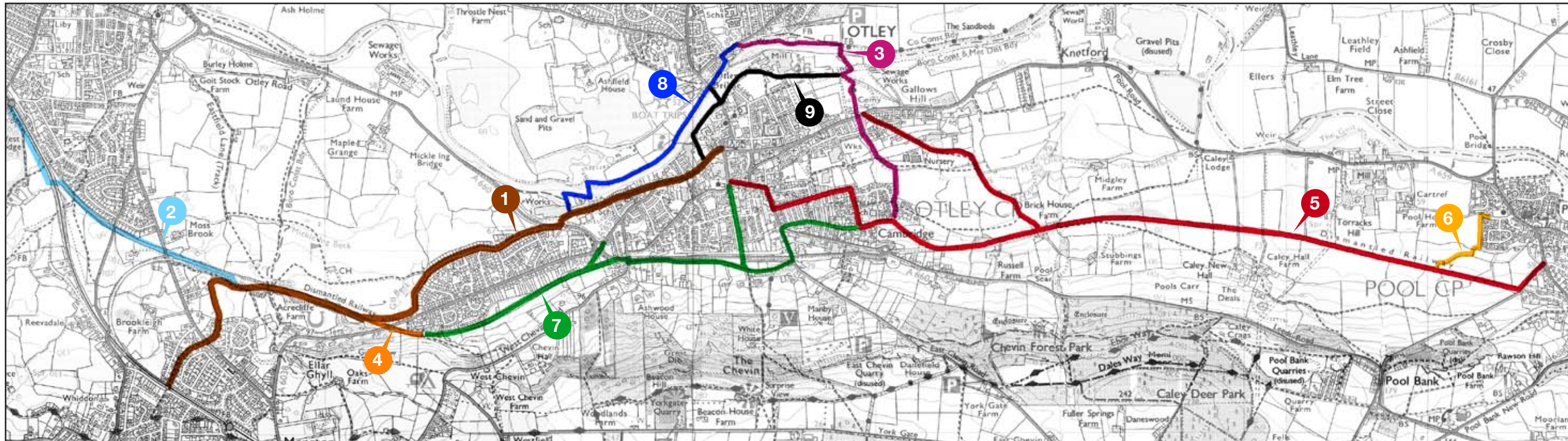
example, might be considered to be the most valuable.

By way of illustration the project might be broken down into the following phases

1	Westbourne Drive Railway Bridge to West Busk Lane via the golf club link. This avoids the expanse of any new bridge but would ideally need cycling improvements to Westgate to link to the town centre.	£350,000
2	County boundary to Burley Station to give the link between Burley and Otley	£500,000
3	Cambridge Drive to White Bridge and Grammar School to give link to school from east Otley	£105,000
4	Bradford Road Bridge to join up with existing railway path	£150,000
5	Otley to Pool Bank including link to East Busk Lane	£505,000
6	Link from railway to Church Lane	£70,000
7	Reconstruction of existing Otley Railway Path and link to West Chevin Road	£190,000
8	Otley Mills to north river bank bridge and link to Prince Henry’s Grammar School	£tbc
9	Garnetts Mill development with financial contribution to walking and cycling improvements over Otley Bridge with link to cycle parking on Westgate.	£tbc

A possible Programme of Work over 4 years

	2010	2011	2012	2013
1 Discussion, revision and adoption of proposals	<div></div>			
2 Agreements to land and planning procedures, stage by stage	<div></div>			
3 Construction of Menston Station to West Busk Lane link	<div></div>	<div>● Easter 11</div>		
4 Link through to Burley Station		<div>● Autumn 11</div>		
5 Cambridge Drive to White Bridge			<div>● Easter 12</div>	
6 Bradford Road Bridge			<div>● Summer 12</div>	
7 Otley to Pool Bank			<div></div>	<div>● Easter 13</div>
8 Link to Church Lane and reconstruction of existing Otley path				<div></div> <div>Christmas 13 ●</div>
Budget for each calendar year	£300,000	£550,000	£550,000	£470,000



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8. Conclusion

This report has set out the proposals in some detail for creating high quality greenway routes linking Pool in Wharfedale, Otley, Menston and Burley in Wharfedale. The disused railway forms the basis for what would undoubtedly be an attractive and popular route, and one which would rank among the best in the country. We recommend that early moves are made to secure the necessary land, submit the appropriate planning applications, assemble the funds and undertake the works for the benefit of the public and a more sustainable transport future for the area.

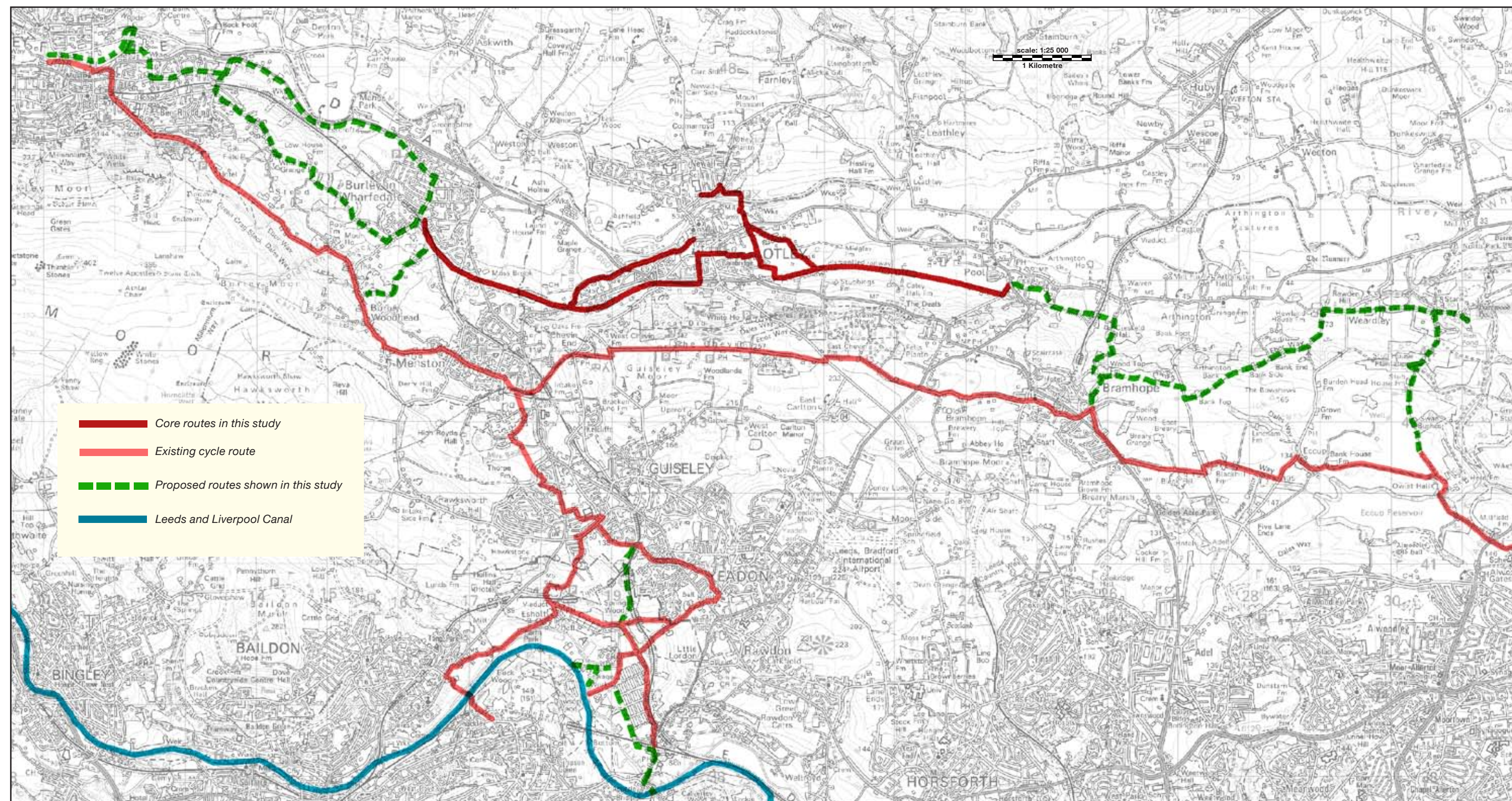
Appendix 1

Possible links to Harewood, to the Leeds and Liverpool Canal and to Ilkley

As well as the core route of Burley in Wharfedale–Otley – Pool in Wharfedale with a link to Menston, the team were asked to sketch out options for routes to Harewood, the canal and to Ilkley.

We have looked at some options for these links, all of which would be valuable for local recreational trips and part of a wider programme to promote cycling on a local tourism and as part of the sustainable transport programme for the area.

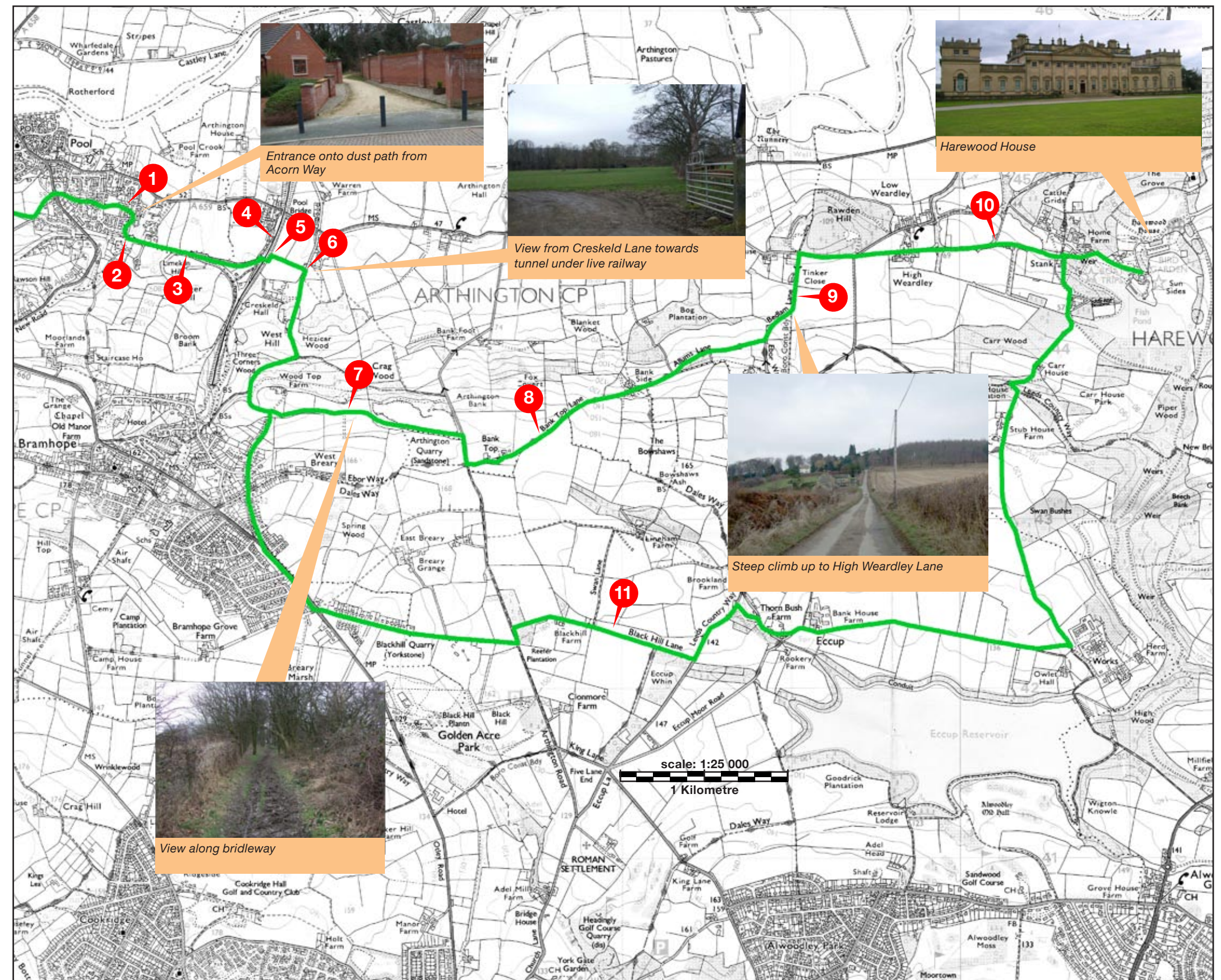
Map 1 shows the routes we have considered along with existing local cycle routes which are signed or promoted by the local authority.



A note on links to Harewood House

Our aim has been to endeavour to recommend a route avoiding the A659, Arthington Lane, and the worst of the terrain to reach Harewood House and its numerous visitor attractions.

1. The route starts in Pool following Swallow Drive which leads through to Acorn Way and the start of an existing recreational path constructed as part of the new development.
2. A locked gate leads to the old railway line via a 4m wide drainage ditch which would make an excellent route through to the east.
3. The railway line carries a rough metal track which would make an excellent base for a route to Harewood. Depending upon the usage of this route by the farm it would need to be constructed as a minor road in order to allow continued farm access.
4. Station Road leads back to the main road.
5. This is the key bridge under the railway which could provide the crucial route to Creskeld Lane.
6. Construct a new field edge path to reach the lane. Creskeld Lane is steep and does carry some traffic, particularly at rush hours, it is though much better than the main road. However if negotiations allowed it a new route through the woods to run around the south edge of the quarry would be ideal.
7. This lane would form the basis of a good route but would need to be completely reconstructed to carry agricultural vehicles all the way through to Black Hill Road.
8. Bank Top Lane climbs gradually as a wide unmetalled track leading through to Alluns Lane and eventually Bedlam Lane.
9. Bedlam Lane drops steeply and then climbs up to High Weardley Lane.
10. Final access into the Estate would need to be negotiated from this point onwards. Note that there are a number of tracks in the area and in particular the route marked to the south to reach the location of the Emerald Set would be of interest to many.
11. Existing local route uses the main road in Bramhope but otherwise would be readily available. Work is needed in Bramhope to provide a safe passage in both directions.

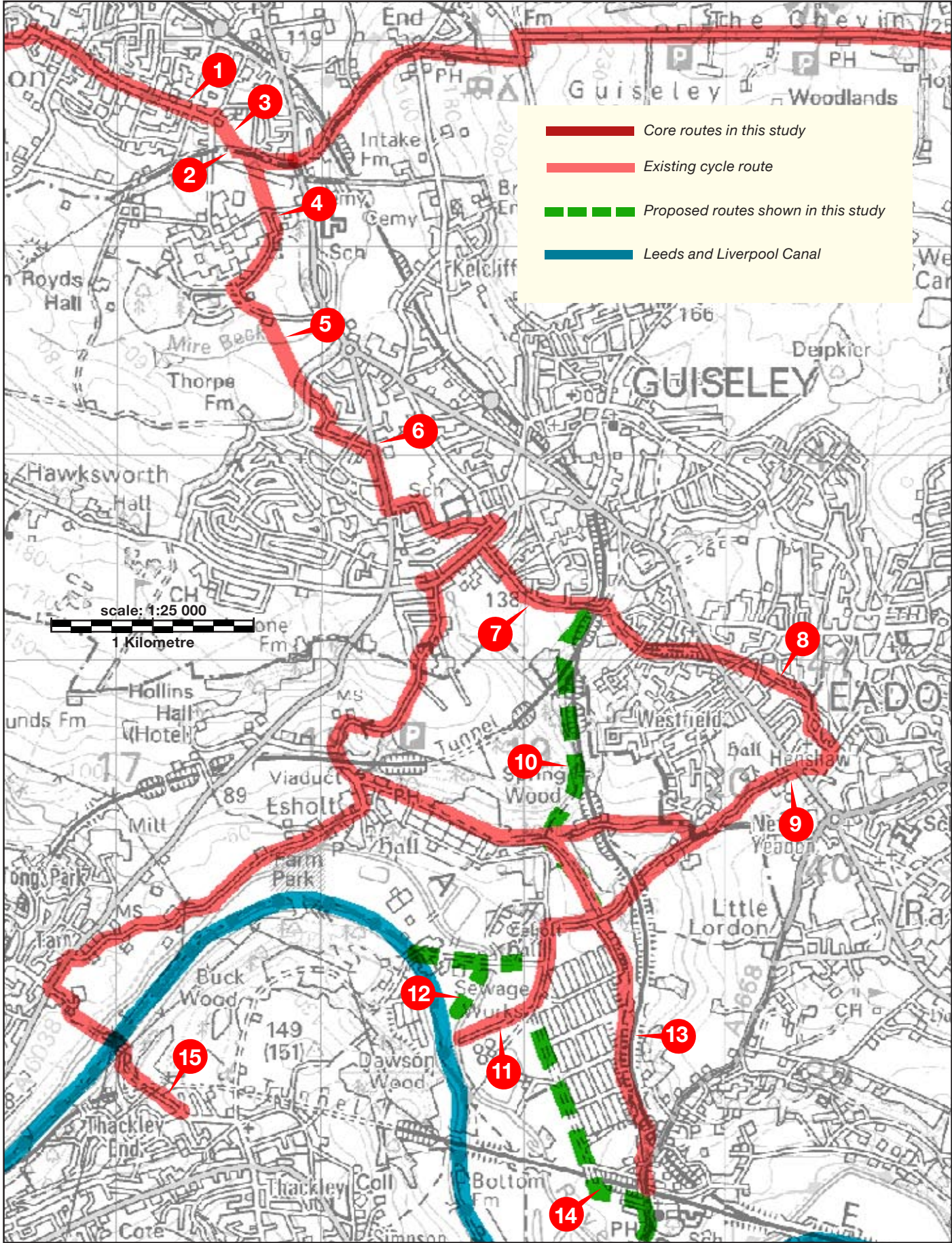


Menston to Yeadon and to the Leeds and Liverpool Canal

This note details how best to connect Menston with the existing railway path at Yeadon and thence to join the Leeds and Liverpool Canal, this being the principle Greenway route in the area.

1. The path from Otley finishes at Westbourne Drive and Menston Station.
2. Follow the residential roads through Menston to reach the Bingley Road and the Menston Drive entrance to the old hospital.
3. Depending on the route to be provided through the hospital it might be more attractive to follow the existing path across the fields to Leathley Avenue and cross the Bingley Road there for a new path reaching to Kingsdale Drive on the new development.
4. This leads through to Jacksons Walk and Ingles Lane following around the south side of the High Royds hospital redevelopment.
5. The existing bridleway which could form the basis of a route through to Yeadon has the disadvantage of going through the farm area and climbing to the top of Thorpe Lane. The landowner might find it more attractive to create a new path direct from Guiseley Drive through to Thorpe Lane opposite the end of New Way.
6. Pick up paths beside the Bradford Road and the existing crossing and route through to school and Back Lane.
7. The Coach Road leads around the edge of the town to cross the mainline railway and join Gill Road for the start of the railway path.
8. This popular railway path was reconstructed in 2008 through to Henshaw Lane.
9. One option is to follow Gill lane with a new crossing of the A65 and follow it all the way down to the Avenue and the Sewage works.
10. Alternatively or additionally reconstruct Springs Road to create a most attractive woodland route down to the valley floor.

11. The existing public footpath route has a disadvantage of following a very narrow route past the sewage works and even narrower bridge over the river to reach the canal.
12. It would be better to use Coronation Avenue to cross the river and then construct a new link beside the river to reach the canal.
13. The existing cycle route follows a path through the woods behind the sewage works. It has the disadvantage of having to follow the main road, Apperley Lane, to reach the canal towpath.
14. Following The Avenue has similar disadvantages.
15. The Buck Lane footpath route is another possibility, when going towards Shipley, but it also uses a section of the main road.



A note on links to Ilkley

The existing cycle route follows Moor Road high above both towns. This is alright for a strenuous workout but it is hardly suitable for a link between the two towns.

A route beside the main road would be the easiest, but the weight of traffic might make it less than pleasant. Alternatively a mid height route may be possible.

And then there is always the train. There is a frequent service between the two towns and space is unlikely to be a problem so near to the end of the journey. For the time being this short train trip could be advertised as the link between the two towns!

